



Draft Bankside, Borough and London Bridge Supplementary Planning Document Executive Committee

No.	Title
Appendix A	Draft Bankside, Borough and London Bridge SPD
Appendix B	Consultation Plan and Consultation Report
Appendix C	Sustainability Appraisal
Appendix D	Equalities Impact Assessment

9 February 2010

**Draft Bankside, Borough and London Bridge
Supplementary Planning Document**

December 2009

Timetable for consultation

Consultation on sustainability scoping report	31 July to 4 September 2009
Publication of draft Bankside, Borough and London Bridge SPD *	1 January 2010
Presentation to Planning Committee for comment	19 January 2010
Presentation to Executive for approval for formal consultation	9 February 2010
Formal consultation on Bankside, Borough and London Bridge SPD, sustainability appraisal and equalities impact assessment	12 February to 26 March 2010
Adopt final version of the Borough, Bankside and London Bridge SPD	June 2011

*An informal draft is available from 1 January 2010. There may be changes to the draft following comments from the Planning Committee and Executive. The formal draft will be available from 12 February and you will be able to make written comments on it for 6 weeks until 26 March 2010. **All comments must be received by 5pm 26 March 2010.**

Contact us with any questions or for copies of this document

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Copies are available at www.southwark.gov.uk/bblbspd, libraries and one stop shops.

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Section 1: Introduction

Bankside, Borough and London Bridge make up a unique area with a long and varied history. It is an important part of central London providing jobs and prosperity that helps makes Southwark successful. It is also an area many people call home and rely on to serve their day to day needs.

When London was first established by the Romans, and later in Shakespeare's day, the part south of the River Thames was an important and very special place. Later it came to be seen as a suburb – somewhere much less important and attractive than the 'real' centre of London on the north bank. This has now changed again. History has come full circle and the south is coming into its own.

Bankside, Borough and London Bridge has been designated an opportunity area in the London Plan and it is clear that there are tremendous opportunities to create an extraordinary world city quarter containing successful business districts, sustainable residential neighbourhoods and world class services.

The continuous, traffic-free walking route along the south bank of the Thames has helped create a chain of high quality visitor and cultural attractions. The south bank is once again the home of London government, not only with the Mayor's headquarters at City Hall but with bases for Transport for London, the London Development Agency and the London Fire and Emergency Planning Authority.

Over the next 15 years a range of development is expected to take place in Bankside, Borough and London Bridge,

particularly new housing and business space. As there is a limited amount of sites available for development we need to make the most of them.

The Thameslink improvements and the Jubilee Line upgrade will make London Bridge station one of the most important transport hubs in the capital. Until now, it has been seen very much as a gateway to places north of the river with most of the people leaving the station in the morning peak heading straight across the bridge to the offices in the City. There is a great opportunity now to make London Bridge a more important destination with developments in and around the station carefully planned so that people living and working there will find it attractive, convenient, lively and friendly and the maximum benefit is achieved for the local area and for London as a whole.

The 306 metre tall mixed use tower at London Bridge known as the 'Shard' is already emerging from the ground now and will take its place as a symbol of the confidence of this part of central London in time for London 2012. The area is already associated with one of the world's leading centres for medical training, research and treatment in Guy's Hospital and the King's College Medical School whose plans will reflect their international prestige with the quality of environment and accommodation that they deserve.

While London Bridge station and its immediate surroundings will undergo a dramatic improvement, other parts of the area around Bankside and The Borough will benefit from more gradual change so that they continue to be among the most

attractive and convenient residential areas in central London. The area has the potential to provide a very high quality of life and a wide range of services locally. The trend towards sustainable transport, particularly walking and cycling, will be continued and opportunities to reduce the impact of motor traffic congestion experienced in some parts of the area will be taken. The highly valued parks, gardens and other open spaces in the area will be protected and improved.

Bankside, Borough and London Bridge share a special relationship with Elephant and Castle as part of London's central activities zone and opportunity areas. Over the coming years they will grow and develop together. It is important that this is well coordinated as what happens in one area will influence what happens in the other. There are opportunities for shared social and economic infrastructure such as sports, leisure and community facilities. Ensuring these areas are well connected will be key to spreading the benefits.

1.1 What does this document do?

This document sets out the overall plan and detailed guidance for how development in Bankside, Borough and London Bridge should occur. By looking at all the issues and opportunities together we can make sure that development happens in the right way that is sensitive to the unique qualities of places and helps make Southwark and London successful. It covers the following topics:

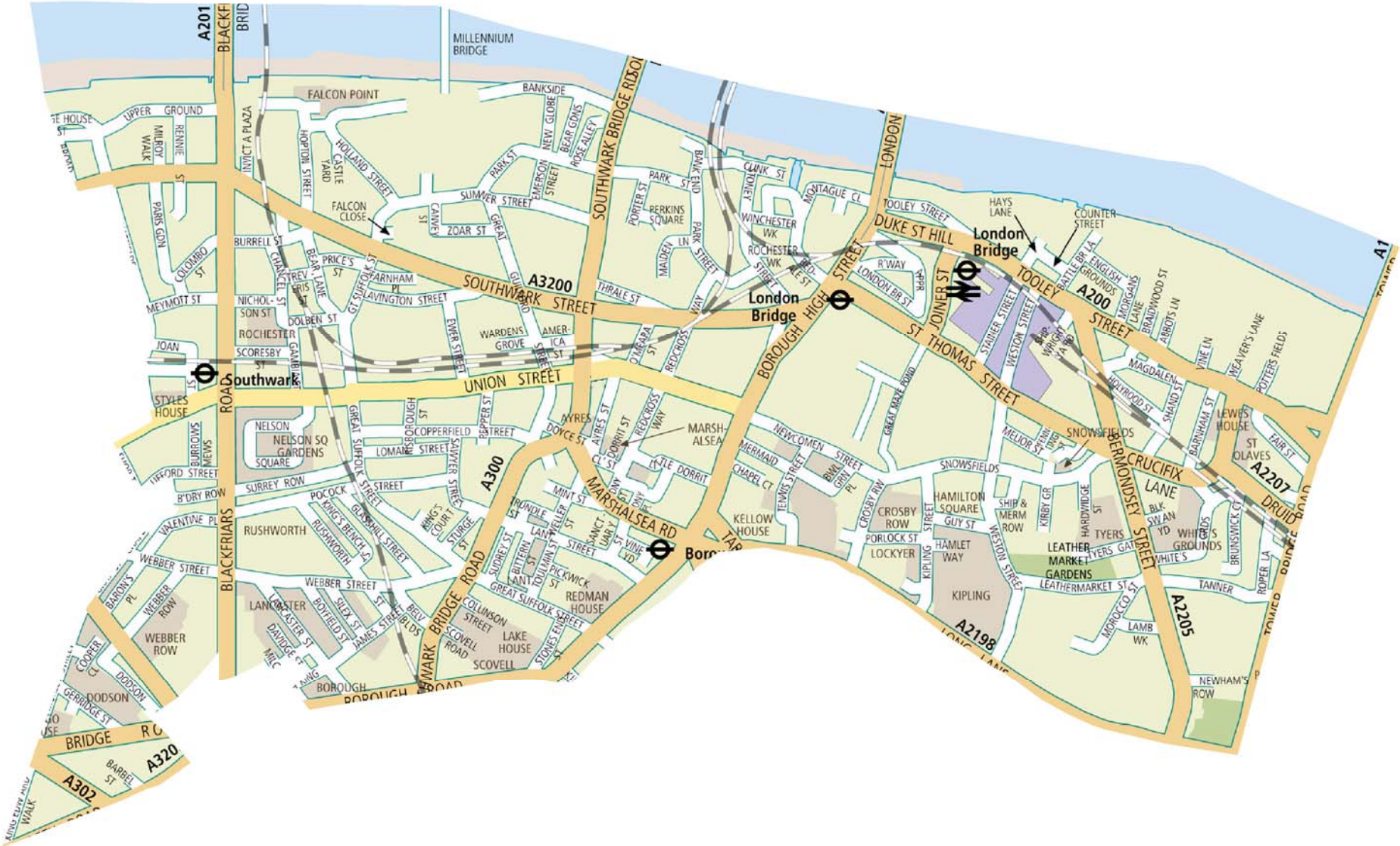
- A vision for how opportunities should be taken.
- What uses should be included in new development and how their impacts will be controlled.
- How the design of development needs to respond to the unique heritage and history of places.
- What heights are appropriate in different locations and the criteria that tall buildings need to meet.
- Improving the look and feel of streets and public spaces, including how easy it is to get around.
- Managing the impact of development on traffic and transport infrastructure.
- Opportunities in the area to respond to climate change.
- The particular contribution that development on certain sites can make to improving places.
- How S106 Planning Obligations will be used to help deliver the facilities and infrastructure needed to support a growing population.

1.2 The area covered by this document

All planning application within the Bankside, Borough and London Bridge Opportunity Area should meet the guidance set out in this document.

Figure 1 shows a map of the area covered.

Figure 1: Area covered by the SPD



1.3 Policy context

This supplementary planning document (SPD) provides detailed guidance on how our current planning policies will be applied in the opportunity area. We have been working closely with the Mayor of London with the intention of it being adopted as Opportunity Area Planning Framework.

The SPD will be used to decide planning applications in Bankside, Borough and London Bridge.

More information on the policy context of this document is set out in appendices 1 and 2.

1.4 Structure of document

- Section 1 sets out the role and purpose of the SPD and its planning context.
- Section 2 explains how Bankside, Borough and London Bridge continue to evolve and the different issues that need to be addressed.
- Section 3 sets out the vision for the area in the future and how different places will change.
- Section 4 provides detailed guidance against the different topics covered.
- Section 5 sets out guidance for how development of key sites can help contribute to improving the area.
- Section 6 explains how we will work with different partners to achieve the vision for the area and how we will use S106 Planning Obligations to secure the improvements needed to support growth.
- A series of appendices provide further background information on the issues and guidance covered in the SPD.

Section 2: The story of Bankside, Borough and London Bridge

2.1 A history of change

The Borough, Bankside and London Bridge area is one of the most historic areas in London. It was settled as far back as 3000 BC. Over the years important events across London and England have shaped the land uses and buildings in the area. Figure 2 provides a timeline of key events that have helped shape the area.

The area's history is reflected in many listed buildings and conservation areas.

2.2 A well connected place

Having access to good transport services and infrastructure is important to support an area's vitality and growth. Borough, Bankside and London Bridge has excellent transport connections to important parts of central London and the wider region by road, rail and River (see Figure 3).

London Bridge station is one of the most important transport hubs in London and one of the busiest in the country with passenger numbers forecast to grow over the coming years.

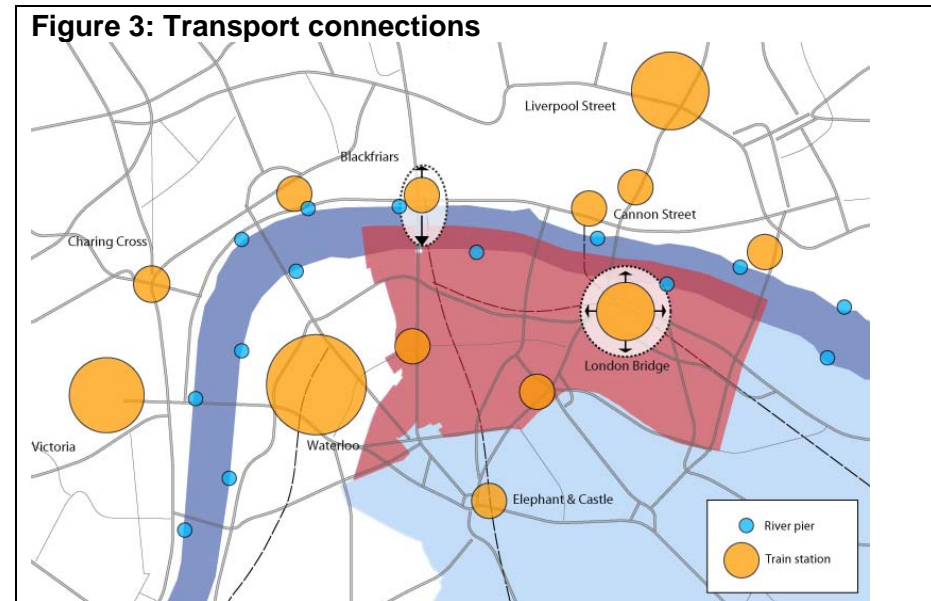


Figure 2: Timeline of events



Improvements in transport such as the Jubilee Line and Thameslink programme are bringing major benefits to the area, including a new entrance to Blackfriars Station from Bankside and remodeled London Bridge Station. This will help unlock the potential of the area.

2.3 A part of London’s and Southwark’s success

Bankside, Borough and London Bridge play an important role in enabling Southwark and London to grow and prosper. The area is located between and linked to major regeneration and activity areas at Elephant and Castle, Waterloo, Vauxhall the City and Docklands. It is part of the Central Activities Zone (CAZ) which includes the West End and the City and contains offices, activities and attractions which are important to London. Its relationship with Elephant and Castle is particularly important as regeneration in both areas will have shared benefits. This is shown on Figure 4.

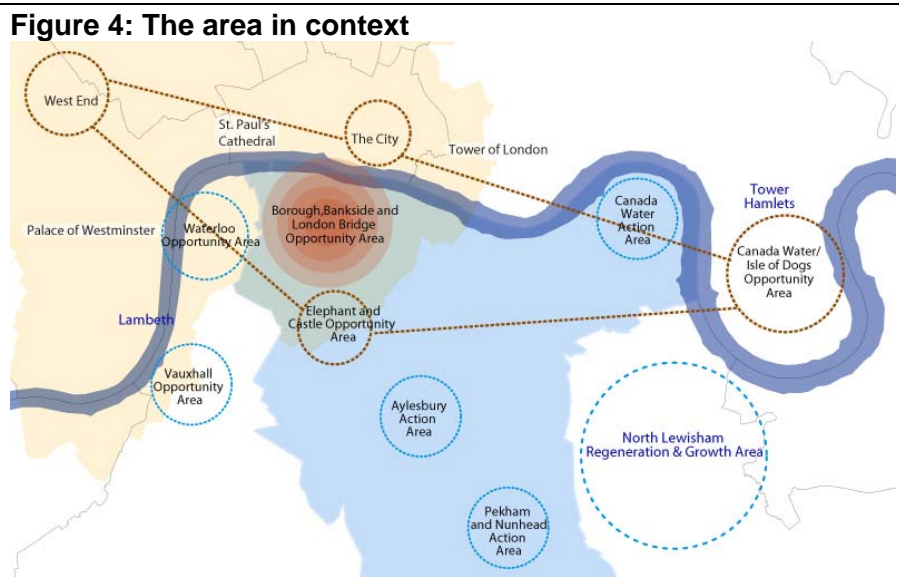


Figure 4: The area in context

Bankside, Borough and London Bridge are the main employment and business areas in Southwark, generating 63% of its wealth. It is home to some of our largest employers. Much of the commercial development is focused around Blackfriars Road, Southwark Street, Borough High Street.

2.4 A place on the map

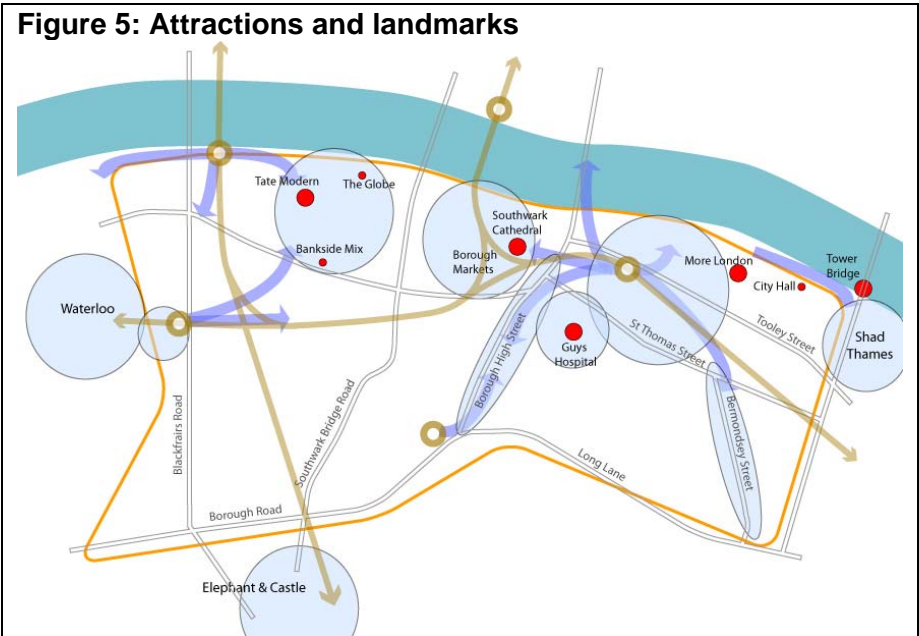


Figure 5: Attractions and landmarks

Bankside, Borough and London Bridge contain some major facilities and attractions bringing thousands of people into the area each day who contribute to Southwark’s economy.

Many of these are arts and cultural venues in the north of the area along the river, which is part of the South Bank Strategic Cultural Area. These include Tate Modern, Shakespeare’s Globe Theatre,

and Southwark Playhouse. The area also contains major institutions including Guy’s Hospital and King’s College.

Alongside the major attractions cafes, restaurants, pubs, bars and nightclubs have established bringing activity to places.

2.5 A place of opportunity

Bankside, Borough and London Bridge have opportunities for further growth and improvements to contribute to the success of Southwark over the next 10 to 15 years.

The biggest opportunities are around London Bridge station and the northern end of Blackfriars Road where there are large sites and a number of high profile schemes planned such as The Shard (see Figure 6). But there are opportunities for development across the area.

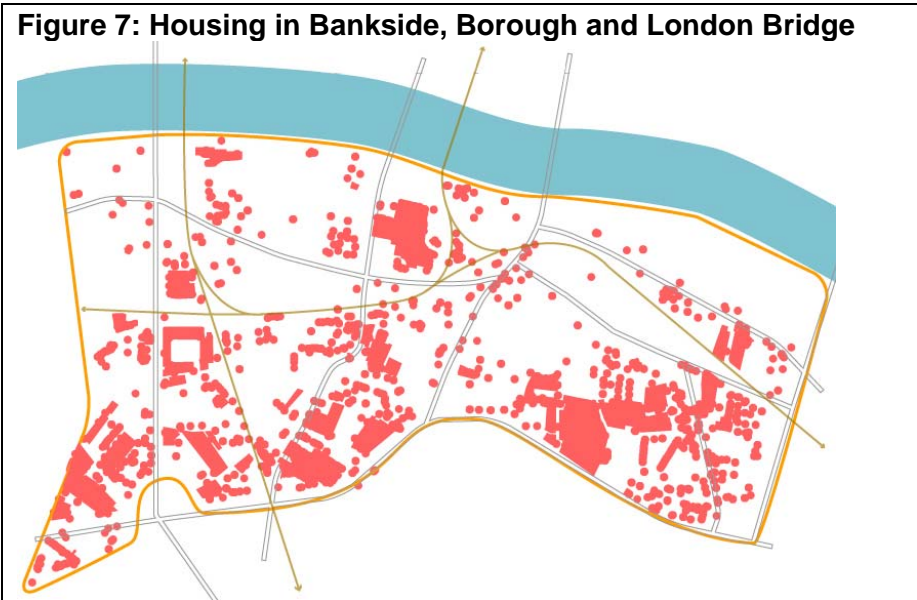


Over the coming years, most of the demand for commercial floor space in Southwark will be in Bankside, Borough and London Bridge.

There is also an opportunity for new homes to be built to help accommodate a growing population.

Development and improvements in the area should also link with the regeneration taking place in Elephant and Castle to ensure that the benefits are shared across the area.

2.6 A place called home



As well as having an important strategic role, around 4,000 people call Bankside, Borough and London Bridge home. Like much of Southwark the population is forecast to grow over the coming years.

It is important that new development caters for local residents and helps increase their quality of life.

While, overall, it is one of the least deprived areas in Southwark and London, there are pockets which still experience high levels of deprivation, particularly in crime, health, access to employment and housing.

There is concern over the impact of development on the quality of residential neighbourhoods. In particular residents have concerns about:

- The disruption caused by construction activity and large numbers of visitors.
- Light pollution and noise from commercial activity.

Within Bankside, Borough and London Bridge there are different places that people identify with. It will be important for new development to be sensitive to the different scale and character of places.

2.7 Environmental challenges

As well as challenges relating to the look and feel of Bankside, Borough and London Bridge and the role that the area plays, there are also important environmental issues that development needs to respond to:

- Flooding – the area is within the River Thames flood zone. Although it is defended to a high level, there is a small risk that the defences may fail and development needs to take this into account. There are also large areas of hard surfaces which, coupled with an old Victorian sewer system, also pose a flood risk.
- Climate change – is affecting everyone, but this area is particularly sensitive as it could lead to an increased flood risk. Given the high concentration of development in the area there

may be opportunities to reduce CO₂ emissions by building energy networks. It will also be important to increase the amount of shading and greenery in the area to help adapt to hotter summers.

- As a very built up area there are fewer opportunities for people to experience nature and wildlife
- Levels of traffic contribute to poor air quality, given vehicular demands in this area. It is located within an Air Quality Management Area.

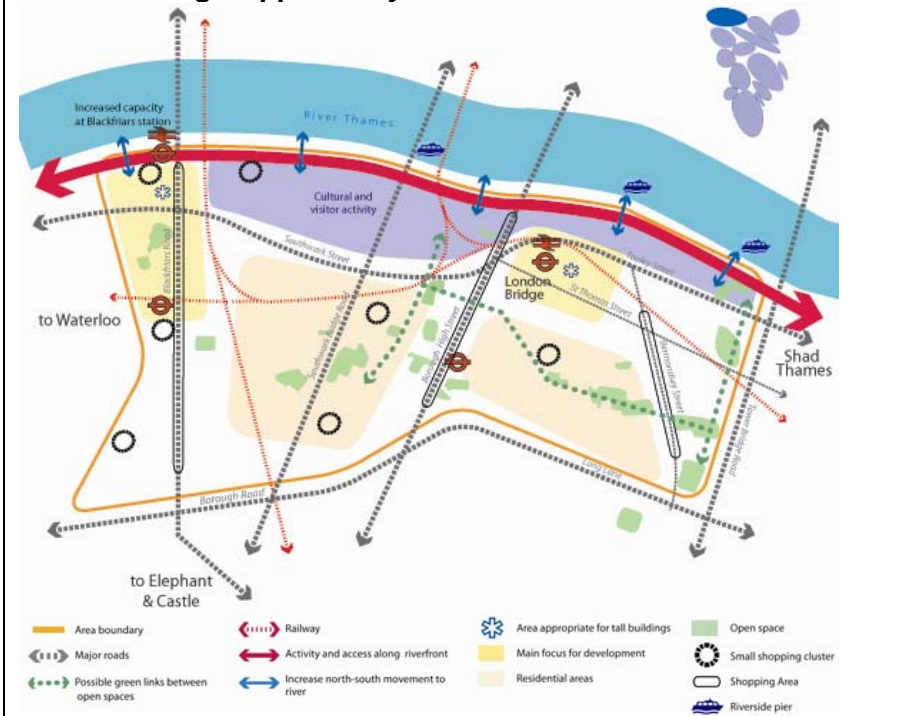
Appendix 3 provides more information on the environmental, social and economic issues and opportunities in Bankside, Borough and London Bridge.

3. What will Bankside, Borough and London Bridge be like in the future?

3.1 The overall approach

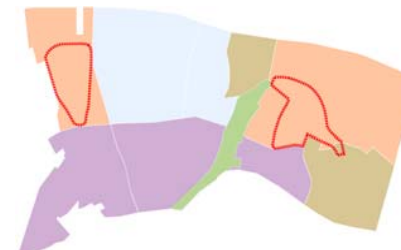
The London Plan and Core Strategy set out an overall strategy for the Bankside, Borough and London Bridge Opportunity Area. This is shown in Figure 8.

Figure 8: Core Strategy key map for Bankside, Borough and London Bridge Opportunity Area



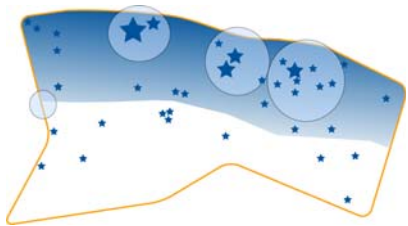
The overall vision is set out below:

Bankside, Borough and London Bridge will be a vibrant, mixed-use area that people want to come to. It will support the economic and business function of central London, promote the success of local businesses and provide a high quality living environment for local people.

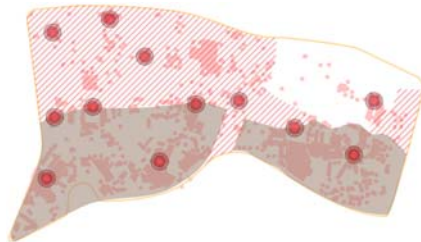


There will be large scale development and improvements, providing:

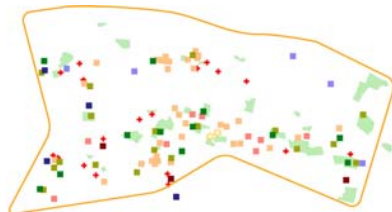
- Over 1,900 new homes
- 400,000sqm – 500,000sqm of additional business floor space, much of which will help meet central London's need for high quality office space.
- Around 25,000 new jobs by 2026 across a range of industries.
- Small local businesses will continue to thrive alongside large global companies



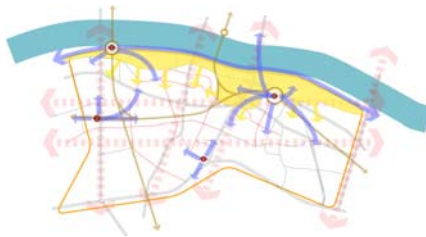
There will be leisure and culture related development that will enhance the uniqueness of the South Bank Strategic Cultural Area and maintain it as a world-class visitor destination.



Bankside, Borough and London Bridge will have high quality sustainable neighbourhoods with a range of housing, shops and facilities to meet the needs of local people.



Local people will share in the benefits of regeneration and investment in the area, such as through employment and training schemes. This will include new and improved community and youth facilities.



Growth in the area will be supported by major investment and improvements to transport infrastructure, including a new entrance to Blackfriars Station at Bankside, increased capacity on Thameslink and a remodelled London Bridge Station.



New development will recognise and enhance the different character and roles of places and respect their past.

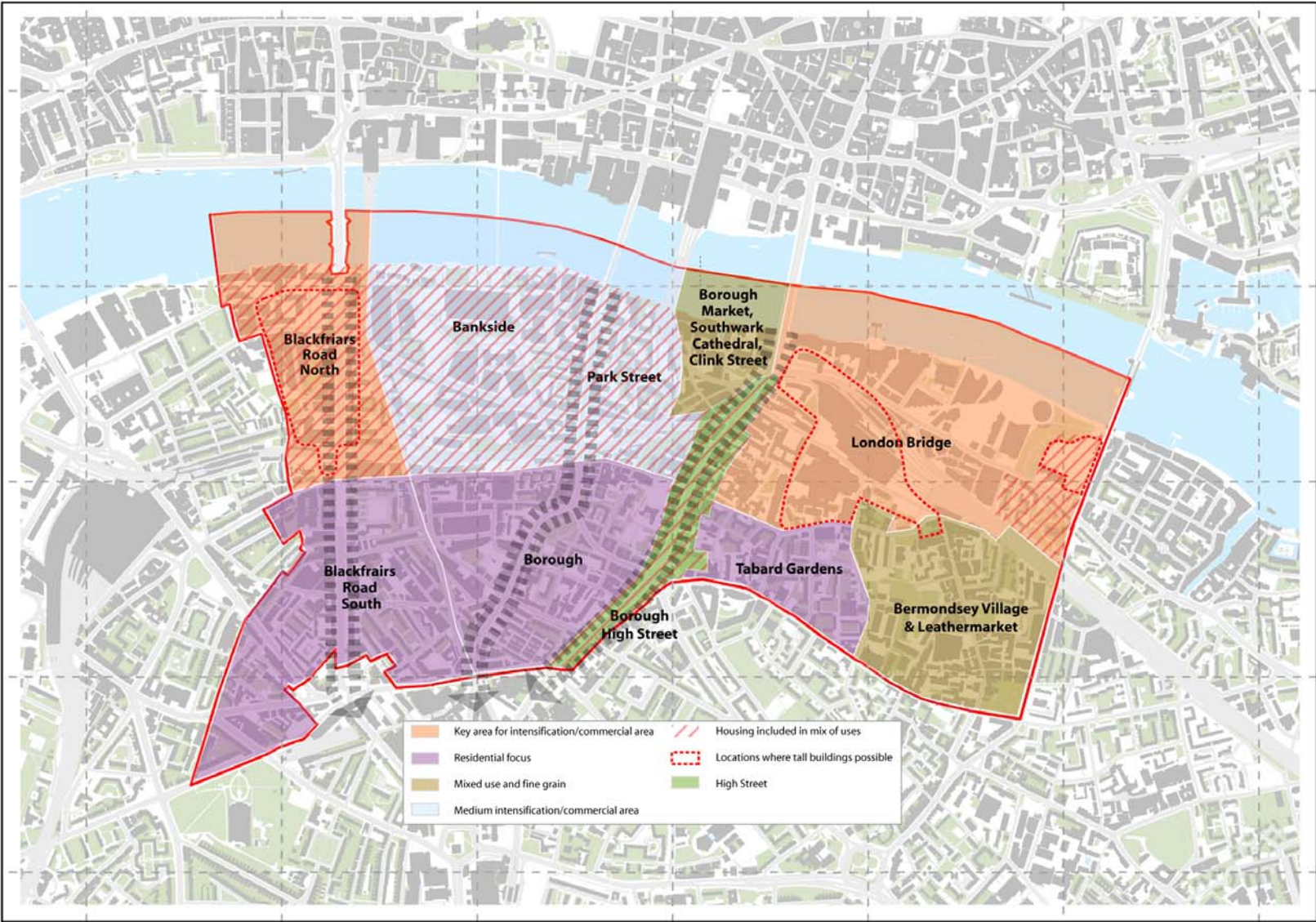


New development will help improve the look, feel and safety of streets and public spaces. Improvements will help people move more easily through the area by walking and cycling.



There will be a good quality network of public spaces and links between them will be improved. New trees and landscaping improvements will make places feel greener.

Figure 9: Approach to change in Bankside, Borough and London Bridge



3.2 What this means for different places

There are development opportunities throughout the area, but the largest developments and the greatest physical change will take place around London Bridge Station, Bankside and the northern section of Blackfriars Road.

Figure 9 illustrates how different parts of Bankside, Borough and London Bridge will change over the next 10 to 15 years.

London Bridge

London Bridge is a thriving commercial area and important part of the central London economy. It will be enhanced with a mix of mainly commercial uses bringing activity to the area and making it a vibrant place throughout the day.

There is a small cluster of sites around the station and we need to make the most of their potential. There will be large scale development and growth on these sites to complement the vibrancy and importance of the centre. This will include excellently designed tall buildings such as The Shard, which will add interest to the skyline, raise the profile of Southwark and create new public spaces.

Major redevelopment of the station to improve access and capacity is key to the development of surrounding development sites.

A redeveloped Guy's Hospital and Kings College campus will provide world-class health, research and education facilities for all of London and improved health services for the local community. It will also have a range of other uses and access improvements to better knit it into the surrounding area.

New development will blend well with the adjoining neighbourhoods, conservation areas and historic buildings.

Much of the development will provide high quality offices, but there will also be major visitor attractions, including a major new cultural facility next to Potters Fields Park. Development will also provide high density housing, hotels, leisure facilities, new shops, cafes and restaurants.

Development will breathe new life to St Thomas Street providing active uses and improvements to the streetscape and railway arches.

New shops will help improve the mix of retail in the area, complimenting the high street function of Borough High Street. There will be at least one large supermarket.

There will be major public realm improvements in London Bridge including better links through the railway viaduct, to the riverside and riverside piers and into neighbouring areas. This will help deal with crowds better. Making better use of the viaduct will also have community and economic benefits.

Blackfriars Road North

Major mixed use development will line the northern half of Blackfriars Road, which will be a major focus for high quality offices and a complementary mix of uses bringing new life to the area. This will include hotels, new retail space and housing. A redeveloped Blackfriars Station will reinforce the area's role as a major gateway into Southwark, which will be complemented by major public realm improvements.

There will be a cluster of tall buildings around the northern end of Blackfriars Road. These buildings will be of exceptional design and will enhance the look of the area and provide new public spaces.

Access to and along the riverside and to and from the new Blackfriars Station will be improved.

Bankside

Bankside will be renowned for its important historic, arts and cultural assets drawing large amounts of visitors. This includes the Tate Modern which will be expanded and improved providing a new entry into Southwark from across the Thames. There will also be high quality office space and new housing. Large scale development will be located north of Southwark Street.

Areas to the south will be more residential in nature with small affordable business space and light industry and more family housing. This will include refurbished space under railway arches.

Active ground floors will bring life to the area and provide new small scale shops, cafes and restaurants. It will be important to get a good balance of uses to protect the amenity of residential areas and to make sure uses like hotels and student accommodation do not dominate.

Public realm improvements including the Bankside Urban Forest (see Appendix 7) will improve public spaces and create better access into areas south of the River and to and from the new Blackfriars Station.

Park Street

This will be a mainly residential area, complemented by new high quality residential development including family housing. There will be some commercial development particularly along Southwark Bridge Road, providing a mixture of large scale high quality offices and space for small businesses.

A new public space will be created on the Crossbones Graveyard. Other improvements will create better access into areas south of the River.

Railway arches will continue in active use for a range of uses and links through them will be improved.

Bermondsey Village and Leathermarket

This will be a vibrant and historic quarter centred around Bermondsey Street. As well as housing there will be a mix of commercial uses providing cultural space, small business space, light industry, independent shops, restaurants, cafes, pubs and bars. Further away from Bermondsey Street will be more residential in character.

Snowsfields will be improved as a local neighbourhood shopping area.

Development will respect the fine grained character and scale of the Bermondsey Street Conservation Area. Routes through the area and between green spaces will be improved

Borough Market, Southwark Cathedral and Clink Street

Its medieval street pattern and important historic assets create a special character that will be protected and enhanced. This will be a vibrant mixed use quarter with homes, small business space, retail and entertainment uses. Evening and night-time uses will be carefully managed to protect the amenity of residents.

Borough Market will be protected and improved. The Stoney Street/Winchester Walk transport and streetscape improvement scheme will improve the environment for pedestrians and cyclists

and better manage loading and deliveries during operation of the market.

New development will be in keeping with the historic character of this part of the Borough High Street Conservation Area, reflected in its historic street pattern.

We will work with Network Rail to ensure the Thameslink project is delivered with minimal disruption and improve the contribution of the railway archways.

Borough High Street

Borough High Street will be a hub of activity and keep its high street function, with a range of shops, facilities and services to meet the needs of local people, office workers and visitors. It is a key link between London Bridge and Elephant and Castle. Development and improvements along the route will help link regeneration and activity across the two opportunity areas.

It will be an attractive street that is easier to move along and cross following a range of streetscape improvements, including improved shop fronts, better paving, reduced clutter and widened footways where possible. There will be better links through the inn-yards to quiet spaces off the high street.

New development will be in keeping with the Borough High Street Conservation Area.

Borough

Borough will continue to be a mainly residential place serving local needs. Development will be mainly infill and redevelopment of vacant sites and will be mainly housing with some small business space and local convenience shopping, cafes and restaurants.

There will be more family homes and development will retain the existing scale.

It will be important to get a good balance of uses to protect the amenity of residential areas and to make sure uses like hotels and student accommodation do not dominate.

Mixed use development will take place along Southwark Bridge Road to bring activity to street level and provide an important link to regeneration at Elephant and Castle.

Railway arches will continue to be used for business space and other uses and links through them improved.

There will be improvements to the look and feel of streets, green spaces and links through the area.

Blackfriars Road South

This will remain mainly residential serving local needs. Development will be mainly housing with some small business space and local convenience shopping, cafes and restaurants. Development will retain the existing scale of the area.

There will be major public realm improvements along Blackfriars Road and improvements to routes through the area. Mixed use development will line Blackfriars Road which will provide an important link to regeneration at Elephant and Castle.

Tabard Gardens

This will be a mainly residential area with some local shops and services. There will be some new development providing high

quality residential accommodation and family homes. Development will be in keeping with the current scale.

Snowsfields will be improved as a local neighbourhood shopping area. Links through the area and between green spaces will be improved.

4. Detailed planning guidance

This section provides detailed guidance against specific topics and will be used to decide planning applications. It covers

- 4.1 – Approach to land uses, including how policies on student accommodation, hotels and employment space will be applied in Bankside, Borough and London Bridge.
- 4.2 – Approach to the design of development in the different parts of Bankside, Borough and London Bridge. This covers the scale, heights and character appropriate in different places and how development will be expected to improve the public realm.
- 4.3 – Approach to addressing transport and traffic issues.
- 4.4 – How policies on climate change, flooding and biodiversity will be applied.

4.1 Land use and activities

All development should aim to diversify and increase the mix of uses in a way that is appropriate to its location.

Figure 10 shows locations where active ground floor uses that bring activity into the area and reinforce its role as a town centre in central London should be included as part of developments.

Applications for commercial uses will need to demonstrate how they avoid environmental and amenity issues that have an adverse impact on residential areas and schools. This includes:

- Avoiding light pollution, in particular impact on sleeping areas from both internal and external lighting.
- Avoiding noise pollution from construction activity and the operation of a building.

- Avoiding crowding and congestion on streets and disruptions caused by construction activity.

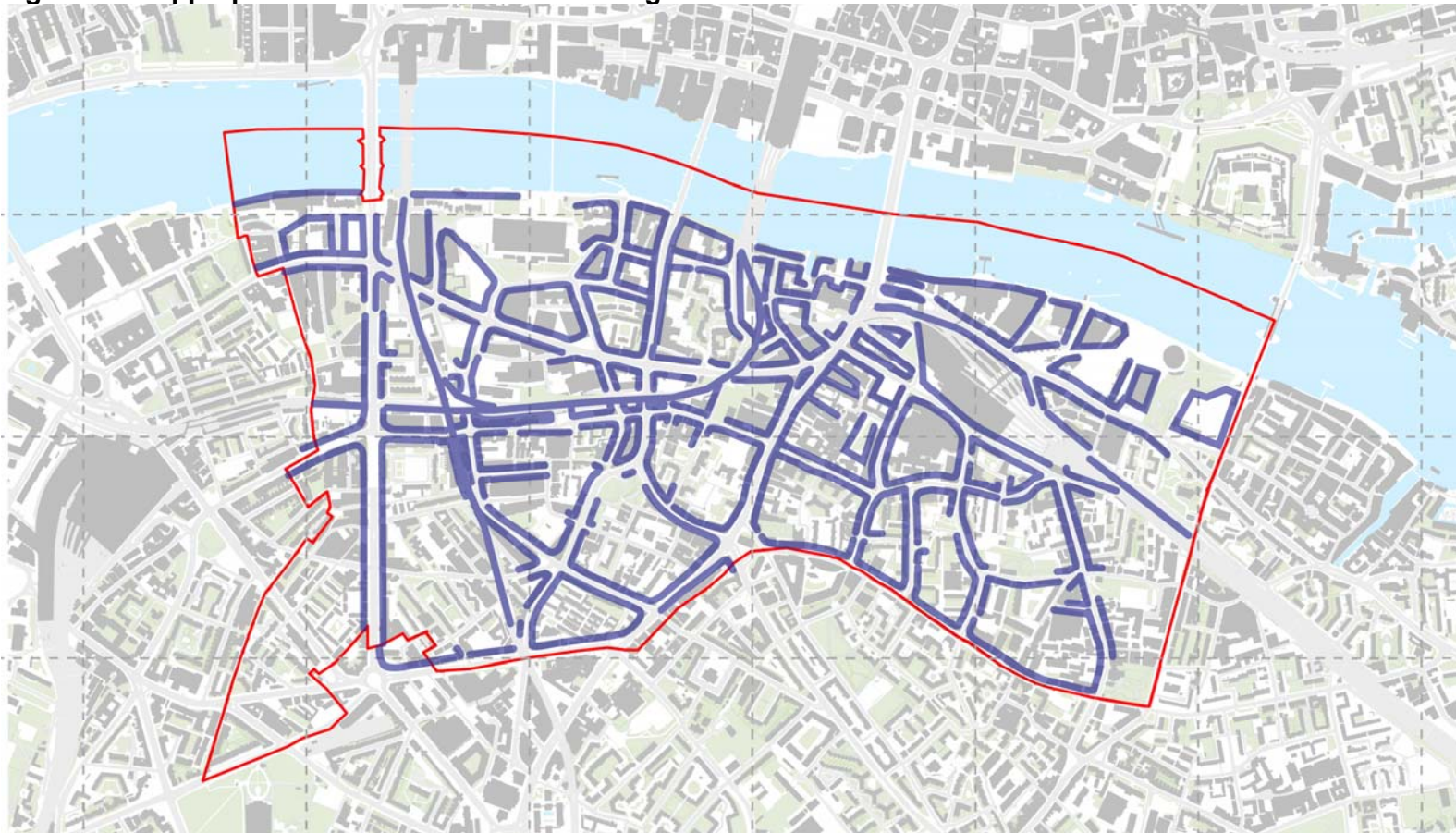
Guidance for specific land uses and activities follows.

4.1.1 Retail

Figure 11 shows the main locations for retail uses in Bankside, Borough and London Bridge.



Figure 10 – Appropriate locations for active frontages



- Active frontages will not be appropriate everywhere. This map identifies suitable locations based on opportunities to bring activity and life into the area.
- Active frontages will not be appropriate in areas where they will create environmental or amenity issues that have an adverse effect on neighbouring housing or in areas that are not appropriate for high volumes of footfall.
- Active frontages should incorporate A class and D class uses. Where other commercial uses are provided these should be designed to provide interest and surveillance to the street.
- Building facades should be designed to add interest and vitality to the public realm. Buildings with active frontages feature frequent doors and windows, with no blank walls, articulation of facades with projections such as bays and porches and, where appropriate, internal uses visible from the outside or spilling on to the street.

This includes the following protected shopping frontages where the mix of retail is controlled.

- Stamford Street
- Hopton Street
- The Cut
- Borough High Street
- Snowsfields
- Waterloo Road
- Great Suffolk Street

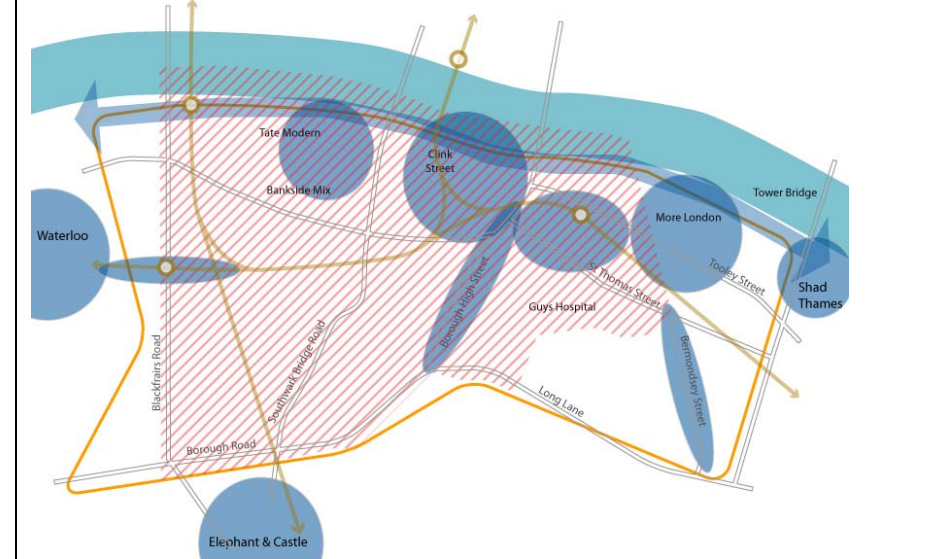
Across the retail frontages, most of the shop fronts are used for non-shop uses such as cafes, restaurants, offices and leisure and provide little choice in terms of shopping. Many of the shops that are there mainly serve the visitor and business population. While other nearby centres such as Canada Water, Elephant and Castle and the West End will continue to be the major shopping destinations, there is an opportunity to improve the mix of retail as part of new developments.

Development should:

- Retain and preferably increase the amount of retail units and floorspace in the building, in particular class A1 uses.
- Where new retail units or active frontages are provided, at least 50% of each frontage should be class A1 units.
- Provide a range of unit sizes including small affordable units suitable for local and independent retailers.
- Developers should work with the local community and retail businesses to secure uses that provide services to local residents.
- Development should help improve shop fronts and the environment of local shopping parades. Where appropriate we will seek S106 contributions towards schemes such as the Improving Local Retail Environments programme.

4.1.2 Restaurants, cafes, drinking establishments and gaming premises

Figure 12: Entertainment areas in Bankside, Borough and London Bridge and the alcohol saturation zone



A mix of restaurants, cafes and drinking establishments help create vibrant mixed use areas across Bankside, Borough and London Bridge (see Figure 12).

There have been concerns over noise and antisocial behavior impacts of restaurants and drinking establishment. This has led to the establishment of an alcohol saturation zone over much of the opportunity area

Proposals for class A3, A4 or A5 uses:

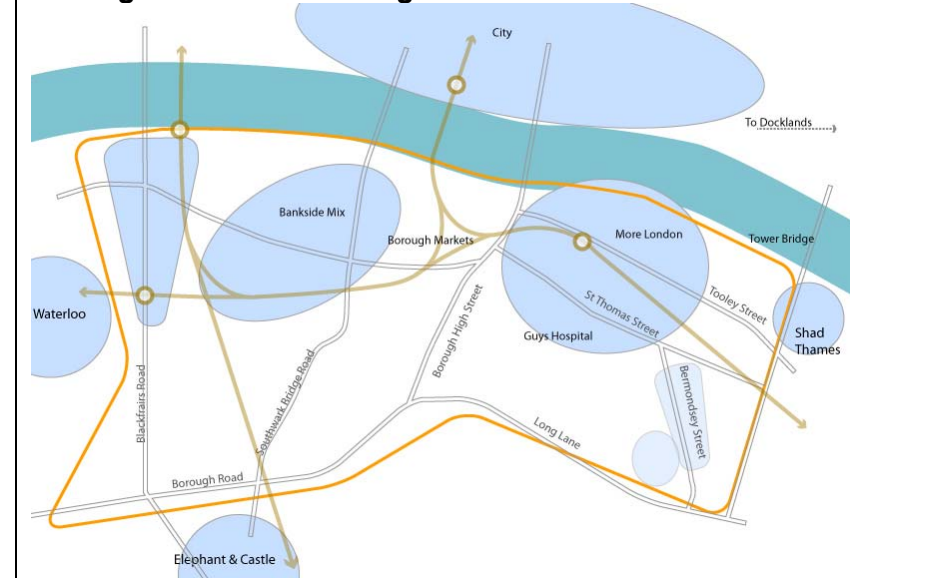
- Will be resisted where they replace class A1 uses.
- Should demonstrate how they will manage potential amenity and nuisance impacts.
- Should be designed to allow the possibility of outdoor seating and dining where this would not cause amenity impacts or obstructions to public spaces. Proposals for street dining tables and chairs on streets will require a Shop Front Licence.
- New proposals for alcohol licences will not be given unless applicants can demonstrate that there will be no increase in crime and disorder, no risk to public safety and no nuisance for residents, in accordance with the alcohol saturation policy.
- Proposals involving more than 1,000sqm of A3, A4 or A5 uses should include a visitor management strategy that explains how the design and operation of the facility will avoid crowding of streets, noise and anti-social behaviour.

There are also concerns with the establishment of gaming premises and amusement centres in the area. Proposals for new gaming premises and amusement centres will be resisted, unless they:

- Are located away from residential areas, public buildings, schools and facilities aimed at families, children and young people.
- Do not result in the loss of a class A retail unit.
- Do not make up more than 5% of shop units in a parade or cluster.
- Are granted a licence in accordance with the Southwark Statement of Gambling Licensing Policy.

4.1.3 Business space

Figure 13: Employment areas in and around Bankside, Borough and London Bridge



Within Bankside, Borough and London Bridge there are a number of important business locations (see Figure 13), in particular around London Bridge, Blackfriars Road North and Bankside where major modern high quality office schemes will provide support to London's economy and bring major investment and job opportunities to Southwark.

Over the next 10 to 15 years there will continue to be demand for business space in the area. It is important that a range of business space is provided to support different types of businesses and allow local businesses to establish.

Within Bankside, Borough and London Bridge:

- We will protect B Class floorspace in accordance with the Core Strategy and Southwark Plan. This includes preventing the loss of B class floorspace to other uses that may create some employment, such as hotels.
- Development should increase the amount of B class floorspace, particularly in London Bridge, Blackfriars Road North and Bankside.
- Additional floorspace on sites in B class use can be for a range of compatible uses, provided these will not jeopardise the continued business use of the site.
- Flexible business space suitable for a range of occupiers should be provided, including light industrial uses that provide services to the local area and creative and cultural industries.
- New space should be designed so that it can be subdivided into units of a range of sizes. Small business units of less than 500sqm will be protected. Development should increase the provision of space for small and medium sized business. S106 Obligations will be used to secure the provision of affordable business space (see Section 5).
- Railway arches should continue in active use for range of uses including small business space and light industrial uses.
- New development will be expected to contribute towards training and employment schemes to help local people access jobs (see section 6).
- Developments will be encouraged to use local supply chains during and after construction of development.

4.1.4 Arts, cultural and entertainment uses

The opportunity area is an important location for arts, culture and creative industries, this includes an expanded Tate Modern and a major new cultural facility next to Potter's Field Park.

Within Bankside, Borough and London Bridge we will support the continued success of a diverse range of arts, cultural and entertainment enterprises and the cultural, social and economic benefits that they offer.

Existing arts and cultural facilities are protected by the Southwark Plan and Core Strategy. Where possible, new development, particularly major schemes, should include space for new creative, cultural and tourism facilities. Developers should work with arts agencies, including the Southwark Arts Forum and Southwark Council's culture team, to identify potential occupiers of new spaces and the impact of proposals on existing provision. In addition to planned facilities, the area would benefit from a new home for the Southwark Local History Library and Cuming Museum.

Proposals for major arts, cultural and entertainment facilities should include a visitor management strategy that explains how the design and operation of the facility will avoid negative impacts on the area in terms of crowding of streets, noise and anti-social behaviour.

4.1.5 Hotels

Recent years have seen a concentration of hotels in the opportunity area, particularly in Bankside (see Appendix 3). While this has brought benefits in terms of job creation and visitors spending more money in the local economy, it is important to retain a good balance of uses and ensure that concentrations of hotels do not impact negatively on the character and amenity of areas. Hotels can reduce the amount of sites available for office development, making it harder to achieve our strategic targets.

Hotels can also have impact on traffic from taxis, coaches and service vehicles. A coach ban has been declared in Bankside in the area behind the Tate Modern (see Appendix 3). Large hotels requiring servicing by coaches will not be appropriate in this area.

New hotel bed spaces are most appropriate in London Bridge and Blackfriars Road North where there is the highest levels of transport accessibility and major mixed use development is expected.

Any proposals for new hotel bed spaces will only be supported where they:

- Do not result in a loss of B Class floor space. We will strongly apply our policies to protect B class uses and ensure we meet our strategic targets for jobs and employment floorspace (see section 4.1.3).
- Will not lead to adverse impacts on the amenity of residential areas from noise, traffic and anti-social behaviour. Proposals for hotel development should include a visitor management strategy that explains how amenity impacts will be avoided.
- Provide public access to ancillary facilities such as restaurants, health clubs and meeting rooms. We will use S106 agreements to secure this. Hotels providing ancillary facilities will not be appropriate in mainly residential areas of Tabard Gardens, Borough and Blackfriars Road South.
- Provide all necessary off-highway pickup and set down points for taxis and coaches and servicing vehicles and will not have a negative impact on highways.

4.1.6 Purpose-built student accommodation

The amount of purpose built accommodation in the opportunity area has grown in recent years, with a particular concentration in Bankside and Borough (see Appendix 3). While there is a need for more student accommodation across London and Southwark, this needs to be balanced with making sure we have enough sites on which to build other types of homes, including affordable and family homes.

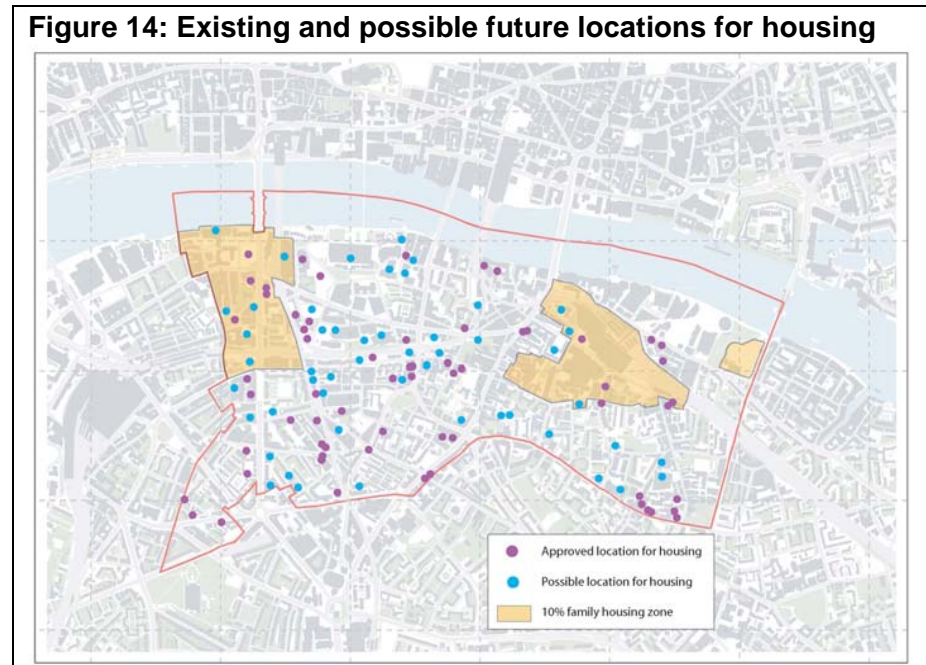
Some new student homes could be acceptable where it is needed to support local higher education institutions, particularly at London Bridge where it would support the Kings College campus, but this should not be the main component of development schemes.

In other areas, further concentration of purpose-built student accommodation that does not support local higher education institutions is not supported, particularly in Bankside and Borough.

Where purpose-built student accommodation is acceptable, Core Strategy Policy 8 requires 35% of accommodation to be provided as affordable housing.

4.1.7 Housing

Figure 14: Existing and possible future locations for housing



Bankside, Borough and London Bridge should have good quality homes where people want to live. Figure 14 shows locations that will help us meet our target for providing at least 1,900 new homes in Bankside, Borough and London Bridge by 2026. Generally, this will be as part of mixed use flat schemes and warehouse conversions. Where possible, all new major development should include some housing.

The Core Strategy sets out the following requirements for developments of 10 or more units in the opportunity area:

- At least 35% of homes must be affordable housing.
- At least 60% of homes must have 2 or more bedrooms.
- Within the areas at London Bridge and the north of Blackfriars Road at least 10% of dwellings should have 3 or more bedrooms (see Figure 14).
- In other parts of the opportunity area, at least 20% of homes should be 3 or more bedrooms.
- A maximum of 5% as studios and only for private housing.
- At least 10% of homes should be wheelchair accessible to create an inclusive environment.
- All developments must meet the following minimum overall floor sizes:

Number of bedrooms	Minimum overall gross internal floor area (sqm)
Studios	36
1 bedroom	50
2 bedrooms	70
3 bedrooms	86
4+ bedrooms	95

Generally, density for both residential and mixed use residential development will need to be within the range of 650 to 1100 habitable rooms per hectare. We do not anticipate needing higher

densities to meet our housing target. Higher densities will only be allowed when developments are of an exemplary standard of design, provide excellent living accommodation and make a considerable contribution to infrastructure and environmental improvements over those required by the standard S106 tariffs (see section 6).

Residential development should meet the standards set out in the Residential Design Standards supplementary planning document

4.1.8 Community facilities

There are a range of community facilities spread in and around Bankside, Borough and London Bridge (see Appendix 3). Growth in the area will increase the demand for community facilities and new development will be expected to contribute to meeting this demand, as set out in Section 6. In particular the area would benefit from:

- Continued support and expansion of youth facilities and service providers, including the Rockingham Centre.
- Implementing phases 2b and 2c of the Whites Ground skate park improvement scheme to create a new youth facility.
- A new community centre at Mint Street.
- A community swimming pool and leisure centre to be built at Elephant and Castle.
- Improvements to play spaces and multi-games courts in parks and residential areas
- From 2018 there will also be the need for a new two form of entry primary school to support a growing population.

Guy's Hospital is also planning improvements to the facilities at London Bridge which will include improvements to local health services.

Where there is a local need, new development should provide flexible community space that can be shared by many groups. It will

be important that developers work with the council and community groups to identify occupiers for the space to ensure they are used effectively and that their use can be sustained and managed.

We will use S106 obligations to secure contributions to the operation, management and maintenance of community facilities to ensure they can continue to be funded over the long-term (see section 6).

We will also work with local schools to improve community access to school facilities through the Extended Schools Programme.

4.2 Built form and urban design

Ensuring high quality design and public realm is a fundamental objective for Bankside, Borough and London Bridge and will be an important consideration in deciding planning applications.

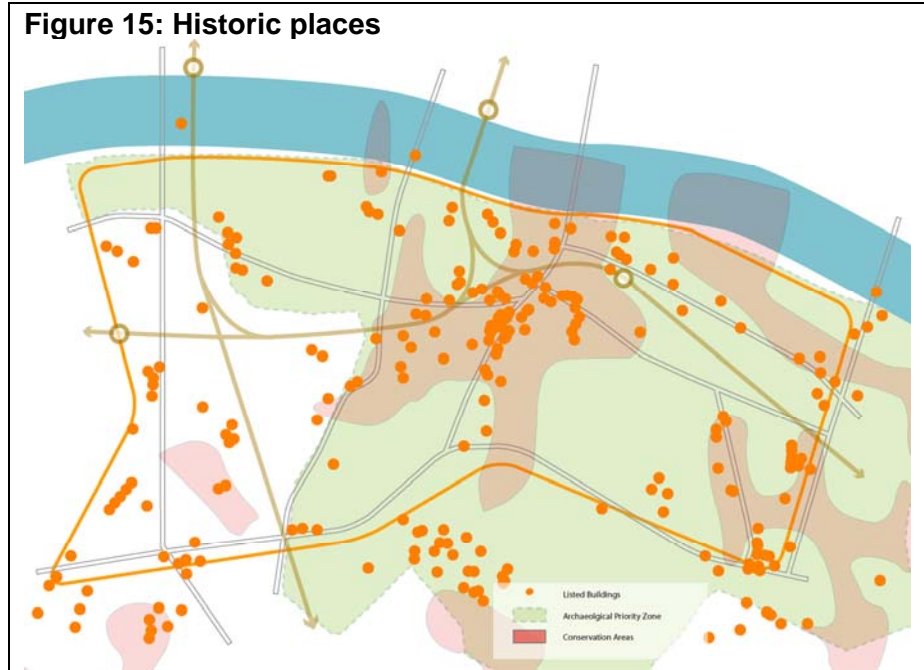
4.2.1 General principles and considerations

Built form

Figure 15 illustrates the conservation areas and listed buildings, monuments and structures in Bankside, Borough and London Bridge. Much of the opportunity area is also covered by an archaeological priority zone.

Most of the conservation areas have conservation area appraisals which explain what is important about each area in heritage terms. These will be adopted as appendix 4 to the SPD.

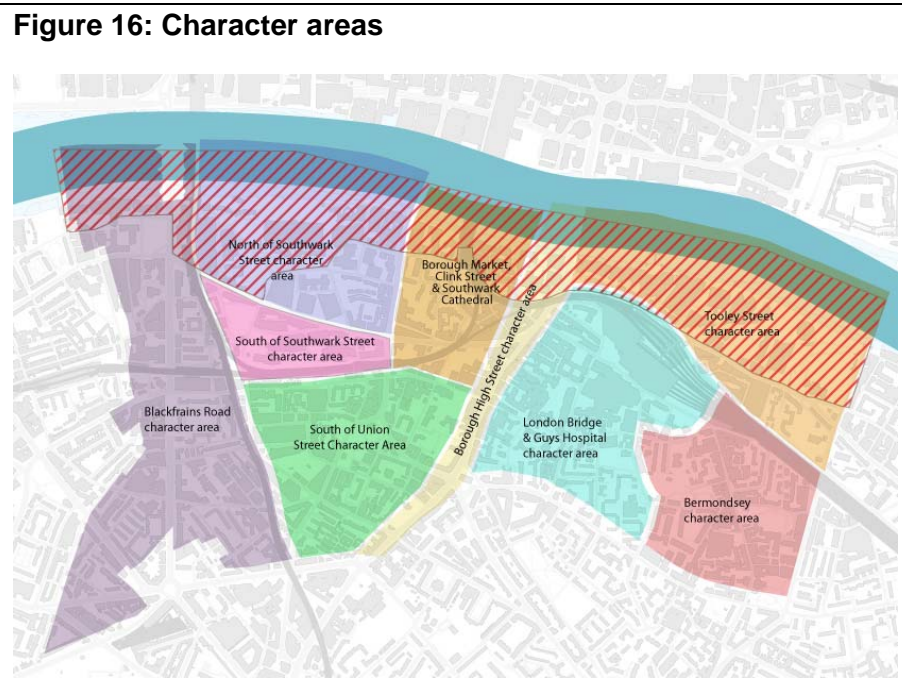
Within the Archaeological Priority Zone planning applications need to be accompanied by an archaeological assessment and evaluation of the site and impact of the proposed development.



We have profiled the character of the different places in Bankside, Borough and London Bridge as shown in Figure 16. The character area appraisals form part of this SPD and are included at appendix 5. They identify how development should respond to the particular character of different places.

The northern edge of the opportunity area lies within the Thames Policy Area, which is a London-wide designation aimed at protecting the special contribution the River Thames makes to London (see fact box).

There are also important views and viewing points across and within the area that help people appreciate important landmarks and the uniqueness of places.



- Be well designed with high quality architecture and urban design.
- Respect and enhance the character of the different places as set out in the relevant conservation area appraisals and character area appraisals. Developments will be required to set out how this has been achieved in their Design and Access Statement.
- Be of a height and design that protects and enhances strategic views and is appropriate to its context and important local views.
- Be sensitive to the many special qualities of the River Thames.
- Consideration should be given to how construction sites can be managed so that they do not detract from the character of places, in particular within conservation areas and around listed building.

We have undertaken a tall buildings study to inform our guidance on appropriate building heights in Bankside, Borough and London Bridge. An outline of the approach to the tall buildings study is set out in Appendix 6.

Fact Box: Thames Policy Area

The River Thames and its riverfront have been given special protection across all of London. The Thames Policy Area in Southwark is shown on the proposals map. Development in this area will be expected to be carefully designed to protect and enhance the River environment and the important contribution it makes to both local people and all of London. This includes its contribution to the history of Southwark and London, its use for recreation and transport, protection and enhancement of the river walkway, and its importance as a habitat and part of the city’s natural cycles.

All development in Bankside, Borough and London Bridge should:

Fact Box: Why consider tall buildings in Bankside, Borough and London Bridge?

The London Plan and Core Strategy have identified that the areas around the north end of Blackfriars Road and London Bridge Station area is a suitable location for tall buildings.

Tall buildings can offer unique opportunities and benefits to the area including:

- **A landmark for Southwark**
As the opportunity area is located along the river front and forms an entrance to Southwark from the City, new tall buildings of exemplary architecture such as the Shard, can provide identifying landmarks that will be clearly visible

from far away, raising the profile of the area and adding interest to the skyline.

- **Focus for regeneration**

New tall buildings can act as a stimulus for regeneration by unlocking the potential of an area and increasing its attractiveness for investment. At London Bridge, they can help revitalise the area south of the viaducts. At Blackfriars Road North they can help bring new life and activity into the area around the new Blackfriars Station.

- **Benefits of investment in the area**

London Bridge and Blackfriars Road North are key areas of major development, activity and business. Tall buildings can help draw visitors and major companies into an area who will spend money in the economy. They require large amounts of investment and construction which will benefit the local economy. As they are such major developments they also act as catalysts for major investment in infrastructure, public realm, transport and community facilities, such as the new bus station being delivered as part of the Shard development.

- **Provide new and improved public realm opportunities**

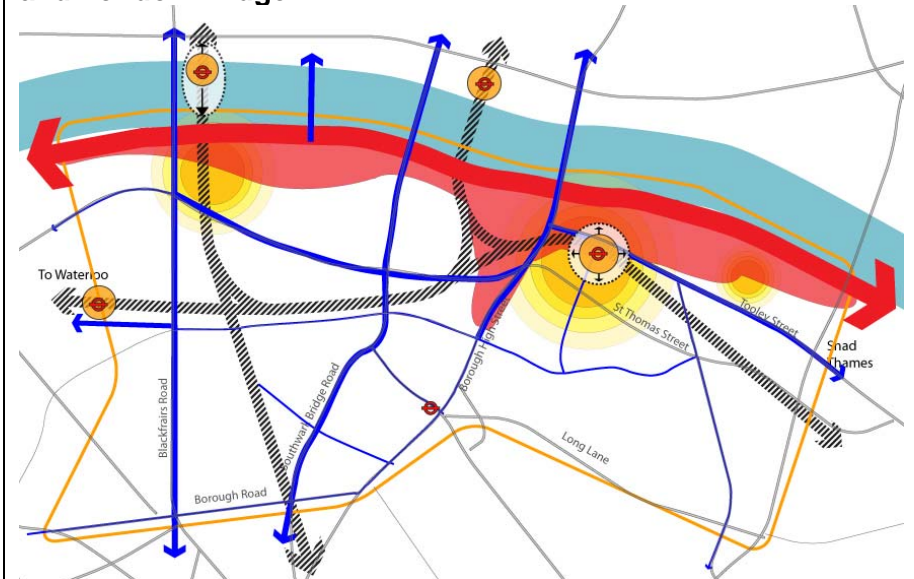
Tall buildings provide the opportunity to release land for new public realm opportunities and provide investment in the existing public realm. Tall buildings could help enliven St Thomas Street with new active frontages and uses, new public spaces and better links into the area south of the viaducts. Tall buildings at Blackfriars Road North could help provide new and improved open spaces, better links through the area and investment in the Blackfriars Road streetscape.

Public realm and movement

The public realm covers the streets and public spaces that make up an area. Often it is what people remember most about an area and has a very important role to play in making places distinct and successful. A good public realm will help attract business to an area, help buffer activity from quiet places and improve amenity in residential neighbourhoods.

At the moment most activity is focused in the area north of the railway viaducts, which act as a barrier. This can cause crowding in streets and spaces, such as outside London Bridge Station and around Borough Market (see figure 17).

Figure 17: Existing movement patterns in Bankside, Borough and London Bridge



The current network of pedestrian routes is also not helping to reduce crowding as they suffer from heavy traffic, are narrow and cluttered, or unpleasant. Currently most people tend to move along a small number of busy routes. This includes the Thames Path which has become congested in sections, such as around Blackfriars Bridge.

Intensification of development in this area will put pressure on the public realm by bringing more people into the area. New entrances at London Bridge and Blackfriars Stations will also change the way that people move around by shifting the focus of activity and the routes that people use to move through the area.

Hoardings and scaffolding from construction activity also detract from the look and feel of the area.

With many businesses in the area there is also a large amount of advertising and building signage of different styles and types which can detract from the street. This is particularly a problem along main shopping areas like Borough High Street.

Bankside, Borough and London Bridge has a network of small, scattered green spaces which provide relief from busy and bustling places. But there is a lack of access to large green spaces and some parts of the area lack trees or greenery.

Many of the green spaces are well used and would benefit from improvement. This will be particularly important to help the open space network accommodate a growing population in the area.



Figure 19 summarises the approach to creating a high quality, safe and attractive public realm that helps create distinctive places. The key principles are:

- Create more space for pedestrians, especially around busy points. This includes reducing street clutter.
- Create new points of activity and interest to encourage movement away from crowded areas.
- Reduce barriers to movement and create a well-connected and legible area that is easy and safe to use. This includes improved crossings of busy roads.
- Roll out the Legible London signage project across the opportunity area.
- Improving links to and between the City, Waterloo, Elephant and Castle, the Riverside and Canary Wharf. Improving links between the opportunity area and Elephant and Castle will be

particularly important to help spread the benefits of regeneration in each area and improve access to shared infrastructure and facilities.

- Create a network of local routes through the area away from busy roads to better link facilities, places and open spaces.
- Provide a network of good quality green spaces that give respite from busy areas. Improve the quality of spaces so that they function better.
- Bring nature into the city through tree planting and the creation of green links and improving habitat.
- Design public and communal spaces to be child friendly with opportunities for informal play and interaction.

Figure 19: Public realm improvement strategy



All development in Bankside, Borough and London Bridge should be designed using these principles. We will also expect development to:

- Be designed to improve community safety and prevent crime by incorporating the principles of Secured by Design.
- Use decorative hoardings that make a positive contribution to the appearance of the site and surrounding area.
- Ensure that advertising and building signage is of a high quality and suitable size and design in relation to the building and area it is located in.

Any proposals or public realm projects relating to the Riverside walk should involve the National Trails Office.

Many improvements to the streets and public spaces in Bankside, Borough and London Bridge have already been achieved or are being rolled out through a number of projects, including:

- Light at the End of the Tunnel.
 - Borough and Bankside Streetscape Strategy
 - London Bridge BID street enhancement plan
 - Legible London signage project
 - Bankside Urban Forest
 - Improving Local Retail Environments (ILRE) programme
- Detailed of each of these is provided in appendix 7. Investment in the area will provide the opportunity for further improvements to streets, public spaces and transport.

4.2.2 Strategy for London Bridge

Character and built form

London Bridge has a varied built form and urban grain, that includes historic 18th and 19th century warehouses and modern office buildings with large floor plates that do not respond to the historic character or original historic street pattern. New development in the area will be expected to:

- Respect and improve the character of its immediate environs and integrate with the historic street pattern.
- Present a high quality frontage to the River Thames and protect and enhance the River environment.
- Create a strong and consistent building line along streets and the Riverside and define the public realm with active frontages.
- Respect and improve the scale and setting of historic buildings and buildings of townscape value, such as the 18th and 19th century row of buildings at the western end of St Thomas Street.
- Be sensitive to the character of the Tooley Street and Borough High Street conservation areas.

Heights and tall buildings

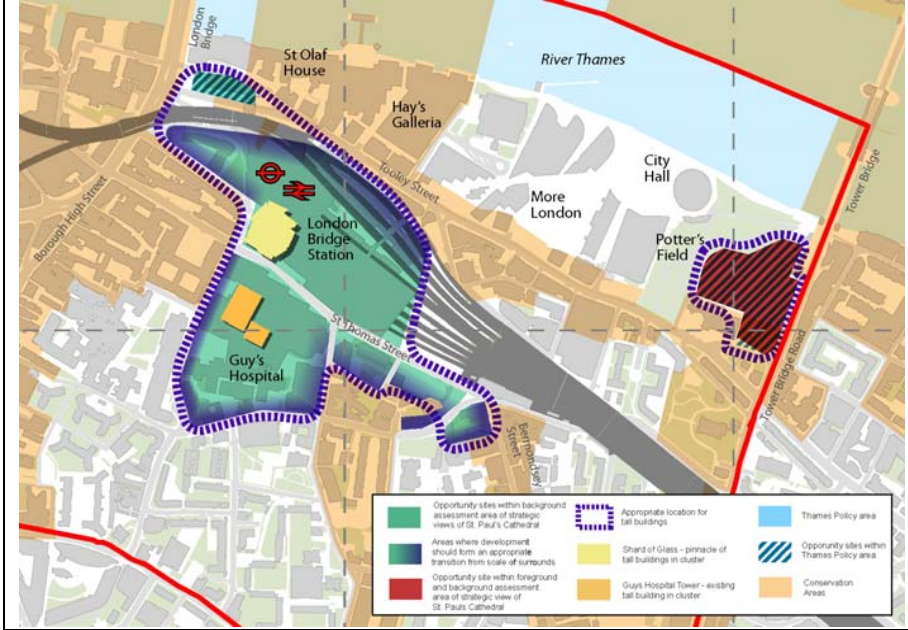
Building heights vary in London Bridge. Generally, development should be in keeping with the predominant building height in the surrounding area and help to create consistent building lines. Figure 20 illustrates the sites that could be developed for buildings significantly taller than their surroundings:

- The Shard has already established a focal point / landmark for London Bridge and should remain the pinnacle of the tall buildings cluster. Other buildings should complement and be appropriate to the scale and prominence of the tower and should not dominate or merge with its silhouette.
- Any proposed tall buildings within this area should be sensitive to the existing surrounds and form an appropriate transition from the height of The Shard to the generally lower height scale of the adjacent conservation areas and the river front development.
- Tall buildings are not appropriate in the Borough High Street conservation area, Tooley Street North and South conservation areas or Bermondsey Street conservation areas.
- Buildings along the Riverfront should respect their setting within the Thames Policy Area. Development on the Potters Field Coach Park should reflect the height of development along the

and not have a negative impact on the views of St Paul's Cathedral.

- Some of the considerations for tall buildings at London Bridge are:
 - Strategic views of St Paul's Cathedral
 - Strategic views of Southwark Cathedral and skyline of the cluster of tall buildings when viewed from the river
 - Setting and views of the Tower of London World Heritage Site
 - Setting, character and views of historic and local context, including listed buildings and structures, conservation areas and local landmarks.
 - An important consideration will be the impact of tall buildings on the character of the Bermondsey Street Conservation Area.
- The tall building study and 3D modelling has indicated that buildings of around 150 metres or taller are likely to become prominent features in the local area and wider London context, visible on the skyline and will require comprehensive consideration of their effect on strategic and local views, their relationship to other tall buildings in the cluster and to the scale and character of the surrounds.
- All proposals for tall buildings will need to satisfy the criteria in section 4.2.12.

Figure 20: Approach to tall buildings in London Bridge



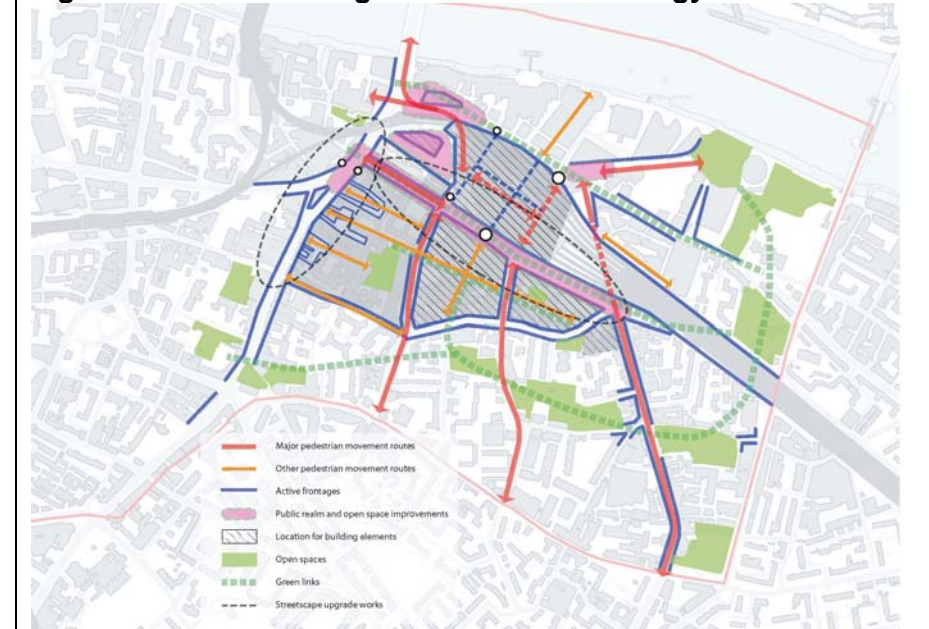
- A new public plaza will be created linking The Shard and London Bridge Place to a remodeled bus station and the entrance to the above ground station.
- Creating more space for pedestrians around station entrances, through street widening, pedestrian priority, reducing street clutter and improving crossings of Tooley Street and St Thomas Street.
- Improve way-finding from station exits.

Public realm

Figure 21 illustrates the public realm strategy for London Bridge. The key aspects of this strategy which development will be expected to help achieve are:

- **Improved station environment and access**
 - Ensuring better circulation and access to and within London Bridge Station and between transport modes. The approach to the public realm responds to changes in circulation and movement patterns resulting from the redevelopment of station.

Figure 21: London Bridge Public Realm Strategy



- **Revitalise St Thomas Street**
 - Revitalising St Thomas Street by improving the streetscape and bringing active frontages to the street. This will include improving the railway archways and using them for active retail and entertainment uses.

- Creating priority for pedestrians and better links from the station and through the viaduct.
 - Improving links east-west along St Thomas Street to take pressure off Tooley Street and the Riverside Walk.
 - New development should be positioned at least 10 metres from the street edge, in order to increase pavement widths along St. Thomas Street. This will assist with pedestrian flows and allow the possibility of tree planting.
 - Providing public space and pedestrian priority at the important junction with Bermondsey Street.
- **Improve links and pedestrian environment**
 - Improve links from the station to key destinations, including Guy's Hospital, Borough Markets, London Bridge, the riverside, More London and Shad Thames.
 - The link between the station and London Bridge is an important and busy route. A high level link across Tooley Street should be provided.
 - Improve local links through the area, in particular link London Bridge to Bermondsey Village, Leathermarket and Tabard Gardens.
 - Improve access from the station through Hay's Galleria to the River Pier. This route will become increasingly important with the creation of new exits from the station.
 - Access to the Riverside Walk could be improved between London Bridge and Hays Lane.
 - Improve pedestrian movement between the level of the riverside walk and pedestrian pathways along bridges to help promote cross-river movement.
 - **Knit the hospital precinct into London Bridge**
 - Improve links into the hospital from the surrounding area, including to and from London Bridge Station and Borough High Street.

- Create a mix of uses that draw activity into the precinct. Improve public access to and the quality of green spaces in the precinct.
- Present a better face to St Thomas Street and Newcomen Street/Snowsfields

Detailed design and public realm guidance for key sites in London Bridge is set out in section 5.

4.2.3 Strategy for Blackfriars Road North

Character and built form

Currently the character around the northern end of Blackfriars Road lacks definition and there is a real opportunity to shape this area and create an entrance to Southwark. New development in the area will be expected to:

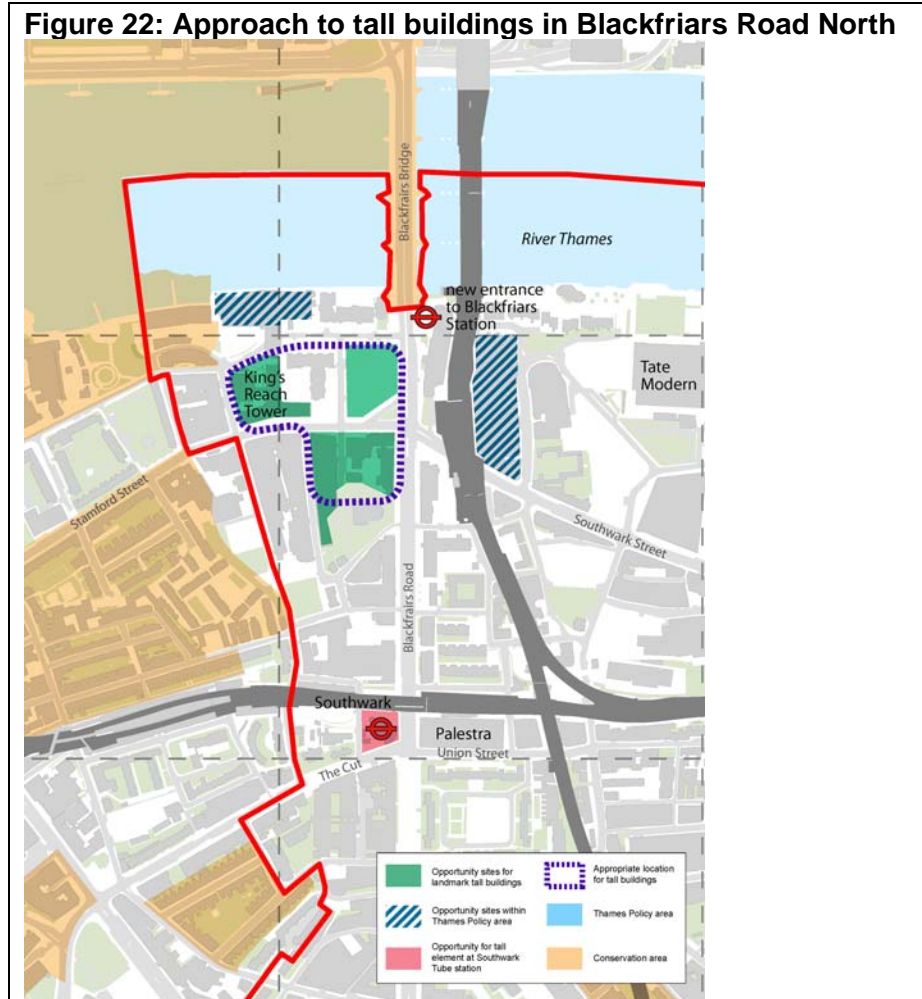
- Present a high quality frontage to the River Thames and protect and enhance the River environment.
- Create definition and a sense of enclosure to streets, in particular Blackfriars Road and Stamford Street. Reinforce the formal axis between the River Thames and St George's Circus.
- Define the public realm with active frontages.
- Infill development should aim to achieve uniform building heights along street frontages.
- Respect and improve the scale and setting of historic buildings and buildings of townscape value. In particular redevelopment of Sea Containers House and King's Reach Tower should reduce the impact of their mass, height and bulk on the small-scale of The Old Barge Alley House Conservation Area.

Heights and tall buildings

Building heights vary in Blackfriars Road North, but the predominant building height over much of the area is three to five storeys. Generally, development should aim to provide consistent building

heights along streets to create consistent building lines. Figure 21 illustrates the sites that could be developed for buildings significantly taller than their surroundings:

- Tall buildings are not appropriate along the Riverfront. The focus of the emerging cluster should be around the key intersection of Blackfriars Road and Stamford Street. Development at this location of landmark importance should be no taller than 165 metres, the height of the schemes already approved for this location.
- A tall building element of a much lower height could be located at the nodal point of Southwark tube station and The Cut / Union Street intersection.
- Between these two ends, development should be closer in scale to the existing character and height in the area.
- Development along Blackfriars Road should avoid appearing as a wall of development within local views of the area.
- Some of the considerations for tall buildings at Blackfriars Road North are:
 - Strategic view from St James’s Park to Horse Guards Road
 - Setting, character and views of historic and local context, including listed buildings and structures, conservation areas and local landmarks. This consideration should also extend to the wider context of the neighbouring Waterloo and South Bank area.
 - Blackfriars Road represents an important vista within the character area. The tight enclosure of medium storey buildings along both sides of the street provides for a series of long and short views.
- All proposals for tall buildings will need to satisfy the criteria in section 4.2.12.



Public realm

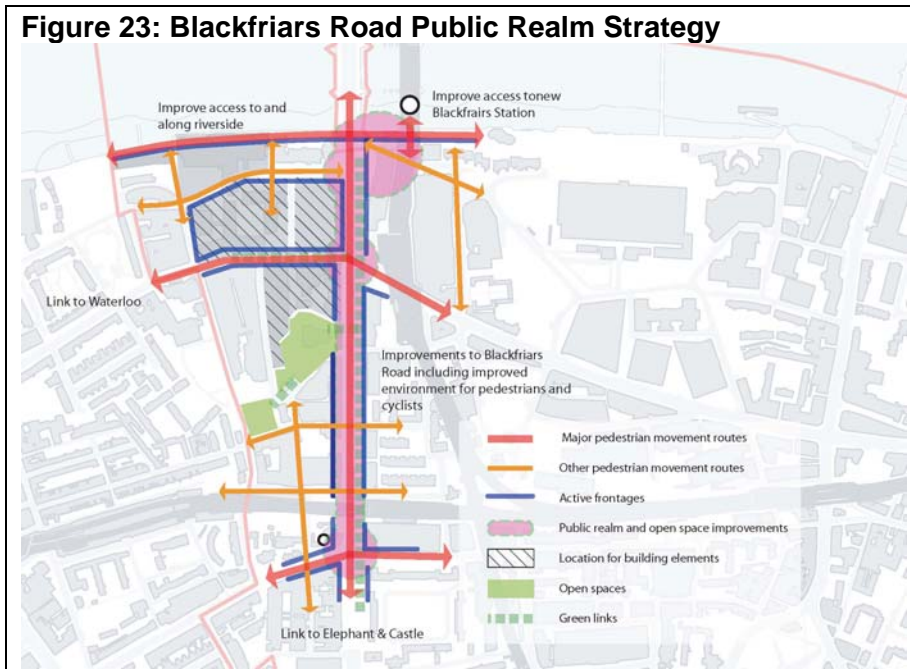


Figure 21 illustrates the public realm strategy for Blackfriars Road North. The key aspects of this strategy which development will be expected to help achieve are:

- **Bring life and activity to key streets**
 - Provide active frontages to encourage people to visit and linger along key routes including Blackfriars Road and Stamford Street.
 - Enliven the Riverfront Walk with active frontages.

- **Improve links and pedestrian environment**
 - Improve the streetscape of Blackfriars Road and Stamford Street including tree planting and seating.
 - Improving access to and along the Riverside between Blackfriars Bridge and Oxo Tower.
 - Improving links to and from the new Blackfriars Station.
 - Improving crossing points along Blackfriars Road and Stamford Street.
 - Improve pedestrian movement between the level of the riverside walk and pedestrian pathways along bridges to help promote cross-river movement.
- **Improve open spaces**
 - Improve links between green spaces and create new or enlarged spaces as part of large developments.
 - Widen the Riverside walk between Blackfriars Bridge and Oxo Tower.
 - Improve Nelson Square Gardens to provide a better quality green space to cater for increased residents and workers in the area.

Detailed design and public realm guidance for key sites in Blackfriars Road North is set out in section 5.

4.2.4 Strategy for Bankside

Character and built form

Bankside has a mixed character with areas of new modern development and areas which still retail the historic 18th century tight and dense urban grain. New development in the area will be expected to:

- Where still existing, maintain the historic street pattern and reinstate when opportunities arise.
- Redevelop unattractive buildings and vacant sites to enhance the character and appearance of the area.

- Present a high quality frontage to the River Thames and protect and enhance the River environment.
- Build to the front building alignment and create a continuous frontages that provide a good sense of enclosure to streets..
- Define the public realm with active frontages.
- Maintain the warehouse character in the areas around Bear Gardens and south of Southwark Street.
- Provide continuation of the mansard roof extensions along Southwark Street.
- Respect and improve the scale and setting of historic buildings and buildings of townscape value, particularly in the Bear Gardens Conservation Area.

Building heights

Building heights vary in Bankside. Development is predominantly low to medium rise (1 to 6 storeys) with the highest development along the main roads. Development should respect the established building height of the area and aim to achieve uniform building heights along street frontages.

Public realm

The key public realm improvements development will be expected to help achieve are:

- **Bring life and activity to key streets**
 - Providing active frontages along key routes including Union Street, Southwark Street, Southwark Bridge Road and the Riverside.
 - Introducing active frontages along the northern side of Union Street in redevelopments of sites between Great Guildford Street and Great Suffolk Street.
 - Bringing activity to the laneway between Union Street and the railway viaduct, linking Ewer Street and Great Suffolk

Street. This could be similar to the restaurant precinct along Isabella Street behind The Cut with shop fronts and restaurants/cafes within railway arches.

- Refurbishing the arches and creating activity in the area between America Street and Union Street.
 - Providing a public space at the intersection of Ewer Street and Union Street and creating pedestrian priority along Ewer Street to link with the new laneway precinct.
- **Improve links and pedestrian environment**
 - Improving pedestrian links from Hopton Street to the new Blackfriars station and from Sumner/Park Streets to Millennium Bridge.
 - Improving Great Guilford Street through creation of shared surfaces to better link Riverside and Southwark Street and cope with increase pedestrian flows and deliveries.
 - Improving pedestrian access through the railway viaducts and improving the link between Great Guilford Street and Flat Iron Square. A key aspect of this is the America Street project.
 - Reestablishing the traditional fine grain street pattern by creating new public access as part of the development of large sites.
 - Widening narrow footpaths throughout the area, particularly along Union Street and Ewer Street.
 - Improve crossing points along Southwark Street.
 - **Improve open spaces**
 - Creating a new park behind Tate Modern as part of the new extension.
 - Bankside Urban Forest will improve public spaces and create better access into areas south of the River and to and from the new Blackfriars Station (see appendix 7). This includes creating a public new space linking America Street and Union Street through the railway arches.

- Develop stronger green links along Union Street through tree planting where appropriate or way finding initiatives to link green spaces together.

4.2.5 Strategy for Park Street

Character and built form

Park Street has a varied character. The Thrale Street Conservation Area has the most consistent and historic character. New development in the area will be expected to:

- Where still existing, maintain the historic street pattern and reinstate when opportunities arise.
- Redevelop unattractive buildings to enhance the character and appearance of the area.
- Present a high quality frontage to the River Thames and protect and enhance the River environment.
- Build to the front building alignment and create a continuous frontages that provide a good sense of enclosure to streets..
- Define the public realm with active frontages.
- Respect and improve the scale and setting of historic buildings and buildings of townscape value, particularly in the Thrale Street Conservation Area.

Building heights

Building heights vary in Parkhouse Street. Development is predominantly low to medium rise (2 to 7 storeys) with the highest development along Southwark Street and the Riverside. Development should respect the established building height of the area and aim to achieve uniform building heights along street frontages.

Public realm

The key public realm improvements development will be expected to help achieve are:

- **Improve links and pedestrian environment**
 - Increasing Park Street's pedestrian access through Borough Market from London Bridge Station.
 - Improving legibility of access through Maiden Lane and Perkins Square to Southwark Bridge Road and Park Street.
 - Improving pedestrian access through the railway viaducts.
 - Improving pedestrian access through large blocks, such as by creating a link through the Crossbones site as part of redevelopment.
 - Improving the northern section of Redcross Way for pedestrians and cyclists.
 - Improving crossing points along Southwark Street and Southwark Bridge Road.
- **Improve open spaces**
 - Creating a new green space on the Crossbones site.
 - Upgrading existing pocket park on corner of Maiden Lane and Park Street to make more inviting open to the street and provide seating closer to the road.
 - Improving green spaces in Gatehouse Square to create a more effective open space for the area. Open up entrance to Southwark Bridge Road.
 - Developing green links through the area linking existing pocket parks to local green chain network.
 - Bankside Urban Forest will improve public spaces and create better access into areas south of the River and to and from the new Blackfriars Station (see appendix 7). This includes extending and improving Flat Iron Square to make it a focal point in the area.

4.2.6 Strategy for Bermondsey Village and Leathermarket

Character and built form

The fine grain character and scale of the medieval street pattern remains largely intact in Bermondsey Village, particularly in the Bermondsey Street Conservation Area. Any development must be in keeping with the following important characteristics:

- Medieval street pattern with long narrow plots, tight building grain and building frontages directly onto the street.
- A series of alleyways and courtyards lead off Bermondsey Street. The narrow arched entrances along Bermondsey Street are a distinctive feature of the area.
- The scale and setting of historic buildings and buildings of townscape value should be protected and improved. The dominant building type is 19th century warehousing of small to medium scale. Historic warehouse buildings should be adapted and reused. New buildings should respect the vertical emphasis and rhythm of the older warehouse buildings.
- Away from Bermondsey Street the area includes a complex network of routes and passages that result in rear elevations of developments being just as important as the frontages.

Building heights

The conservation area is quite sensitive to buildings heights and the impact of tall buildings on the appreciation of the area needs to be carefully considered, in particular the view down Bermondsey Street.

The predominate building height is 2 to 5 storeys and 5 to 7 storeys on sites fronting Tower Bridge Road.

Public realm

The key public realm improvements development will be expected to help achieve are:

- **Bring life and activity to streets**
 - Including active frontages in new development fronting the public realm.
 - Improving Snowfields as a local neighbourhood shopping area. Upgrading footpaths and shop fronts building to create pedestrian-focused area and extending active frontages to corner with Bermondsey Street.
- **Improve links and pedestrian environment**
 - Improving crossing points along Long Lane and Tower Bridge Road.
 - Improving the pedestrian environment along Bermondsey Street, Snowfields, Leathermarket Street and Tanner Street, providing high quality landscaping and street furniture.
 - Improving network of local routes through the area linking London Bridge Station/St Thomas Street with Long Lane and Tabbarad Gardens.
- **Improve open spaces**
 - Improving St Mary Magdalen Churchyard
 - Completing the final stage of Tanner Street Park refurbishment including new planting, seating and playground at south-eastern end of the park.
 - Improving green spaces as assets for local community with a range of facilities.
 - Creating a green link between open spaces to improve access to and between them.

4.2.7 Strategy for Borough Market, Southwark Cathedral and Clink Street

Character and built form

A narrow medieval street pattern and tight fine urban grain characterise this area, which is covered by the Borough High Street Conservation Area. Any development must be in keeping with the following important characteristics:

- Tight containment of the narrow medieval streets by continuous frontage of buildings set right against the street.
- Plot widths and building scales vary.
- Brick warehouses, cobble streets and the railway viaducts are characteristic elements. Historic warehouse buildings should be adapted and reused. New buildings should respect the vertical emphasis and rhythm of the historic warehouse buildings.
- Utility and service areas should be kept behind the street frontages, accessed from the rear or through narrow passages under and between buildings.
- The scale and setting of historic buildings and buildings of townscape value should be protected and improved.
- Present a high quality frontage to the River Thames and protect and enhance the River environment.
- Ensure outdoor advertisements and signage does not intrude on the special character and appearance of the area.

Building heights

There is a range of building heights throughout the conservation area. The predominant building height is 3 to 5 storeys and 5 to 6 storeys along the Riverfront. It is important to retain the variation of building heights that is characteristic of this area.

Public realm

The key public realm improvements development will be expected to help achieve are:

- **Maintaining life and activity**
 - Retaining active frontages fronting the public realm.
 - Re-activating frontages and areas outside market hours
- **Improve links and pedestrian environment**
 - Improving way finding through the tight street network.
 - Improving pedestrian access through the railway viaduct where opportunities exist.
 - Incorporating better lighting.
 - Reducing clutter on streets and unifying street furniture.
 - The Stoney Street/Winchester Walk transport and streetscape improvement scheme will improve the environment for pedestrians and cyclists (see appendix 7).
 - Implementing final phase of the Cathedral Square improvement scheme to improve landscaping, openness and access to the Riverside.

4.2.8 Strategy for Borough High Street

Character and built form

The character of the Borough High Street owes much to Roman occupation and the medieval period when it was used as a primary route into the City. It is covered by the Borough High Street Conservation Area. New development in the area will be expected to:

- Maintain a strong and continuous building line along the street and retain the long narrow medieval plot pattern.
- Maintain and improve the environmental of the coaching inn-yards the run off Borough High Street.

- Protect and improve historic buildings and buildings of townscape value.
- Incorporate strong vertical elements along facades and echo classical principles of proportion and decoration characteristic of 18th and 19th century commercial development.
- Keep utility and service areas behind the street frontages, accessed from the rear or through narrow passages under and between buildings.
- Ensure outdoor advertisements and signage does not intrude on the special character and appearance of the street.

Building heights

From Southwark Street to Union Street building heights range from 3 to 5 storeys. South of Union Street building heights range from 5 to 6 storeys. It is important to retain the variation of building heights that is characteristic of this area.

Public realm

Figure 21 illustrates the public realm strategy for Borough High Street. The key aspects of this strategy which development will be expected to help achieve are:

- **Maintaining life and activity**
 - Retaining active frontages fronting the public realm and improve the look of shop fronts.
 - Creating more active frontages and points of interest at the south end of Borough High Street to draw people down the street.
- **Improve links and pedestrian environment**
 - Improving footpaths and reducing footpath clutter.

- Improving crossing points, particularly at St Thomas Street, Southwark Street Union Street/ Newcomen Street, John Havard Library and Marshalsea Road.
- Widening footways where possible to allow higher pedestrian flows. This could be a scheme similar to Walworth Road.
- Improving link across fork of Borough High Street between 32-34 Borough High Street and Southwark Street.
- Opening movement through existing coaching inn-yards from Borough High Street into King's College and Guys Hospital.
- Having a consistent approach to paving.

4.2.9 Strategy for Borough

Character and built form

Bankside has a mixed character with fragments of the original 18th century terrace street pattern remaining. New development in the area will be expected to:

- Where still existing, maintain the historic street pattern and reinstate when opportunities arise.
- Redevelop unattractive buildings and vacant sites to enhance the character and appearance of the area.
- Create a good sense of enclosure to streets and help create consistent building lines along streets.
- Ensure boundary treatments animate the street edge, providing active frontages in appropriate locations.
- Sensitively re-use historic warehouse buildings.
- Respect and improve the scale and setting of historic buildings and buildings of townscape value, particularly in the Union Street and Kings Bench conservation areas.

Building heights

Building heights vary in Borough, but are generally under 6 storeys. Development should respect the established building height of the area and in-fill development should aim to create uniform building heights along street frontages.

Public realm

The key public realm improvements development will be expected to help achieve are:

- **Bring life and activity to key places**
 - Providing active shop frontages at the corner of Great Guildford Street and Southwark Bridge Road to draw pedestrians into Great Guildford Street.
 - Providing active uses at street level along Great Southwark Street to strengthen the link to Elephant and Castle.
- **Improve links and pedestrian environment**
 - Improving pedestrian movement along Great Guildford Street by widening the footpath, removing unnecessary street furniture and creating pedestrian priority areas or shared zones where appropriate.
 - Improving the junction of Southwark Bridge Road and Great Suffolk Street.
 - Improving the southern section of Redcross Way for pedestrians and cyclists.
 - Upgrading route from Borough High Street to Redcross Way and Southwark Bridge Road through Little Dorrit Park.
 - Improving pedestrian access through the railway viaducts.
 - Creating better pedestrian corridors to provide alternative north-south routes to Borough High Street.
- Re-establishing the traditional fine grain street pattern by creating new public access as part of the development of large sites.
- Improve crossing points along major roads such as Southwark Bridge Road, Borough Road and Marshalsea Road.
- **Improve open spaces**
 - Improving Mint Street Park by opening the park open to the street, greening over of redundant road-heads and improving play facilities, landscaping and seating.
 - Creating a green corridor linking Little Dorrit Park and the St George the Martyr churchyard on Tabard Street and north-south between Mint Street Park and the new Crossbones open space.
 - Improve Little Dorrit Park to improve pedestrian links between Borough High Street and Southwark Bridge Road via Redcross Way.
 - Bankside Urban Forest will improve public spaces and create better access into areas south of the River (see appendix 7).
 - Providing good quality green spaces on estates, including spaces for food growing.

4.2.10 Strategy for Blackfriars Road South

Character and built form

Blackfriars Road South contains an eclectic mix of buildings of varying architectural merit. Little remains of the historic urban form. New development in the area will be expected to:

- Where still existing, maintain the historic street pattern and reinstate when opportunities arise.
- Redevelop unattractive buildings and vacant sites to enhance the character and appearance of the area.

- Create a good sense of enclosure to streets and help create consistent building lines along streets, in particular along Blackfriars Road to reinforce the formal axis between the River Thames and St George's Circus.
- Respect and improve the scale and setting of historic buildings and buildings of townscape value, particularly in the Union Street and Kings Bench conservation areas.

Building heights

Building heights are predominantly 2 to 4 storeys, with heights up to 8 storeys along Blackfriars Road. Development should respect the established building height of the area.

Public realm

The key public realm improvements development will be expected to help achieve are:

- **Bring life and activity to key streets**
 - Providing active frontages to Blackfriars Road. Opening up existing private forecourts and service accesses to improve the public realm along Blackfriars Road.
- **Improve links and pedestrian environment**
 - Improving crossing points across Blackfriars Road and Borough Road.
 - Improving the streetscape to remove clutter, level uneven footpaths, improve wayfinding and ensure consistent street furniture is used.
 - Improving pedestrian access through the railway viaducts.
 - Re-establishing the traditional fine grain street pattern by creating new public access through large sites.
 - Improving St Georges Circus and Borough Road to give more priority to pedestrians and better landscaping (details

are set out in section 5.10 of the Elephant and Castle Enterprise Quarter SPD).

- **Improve open spaces**

- Improving links to green spaces such as Nelson Square Gardens.
- Providing good quality green spaces on estates, including spaces for food growing.

4.2.11 Strategy for Tabard Gardens

Character and built form

Tabard Gardens retains elements of its historic urban grain of terraced streets, particularly along Snowfields and Weston Street. In parts the character has eroded following war damage and the building of large estates. New development in the area will be expected to:

- Where still existing, maintain the historic street pattern and reinstate when opportunities arise.
- Redevelop unattractive buildings and vacant sites to enhance the character and appearance of the area.
- Create a good sense of containment to streets and consistent building lines.
- Respect and improve the scale and setting of historic buildings and buildings of townscape value.

Building heights

Development should respect the established building height of the area. The predominant building height is 2 to 5 storeys.

Public realm

The key public realm improvements development will be expected to help achieve are:

- **Bring life and activity to key streets**
 - Providing active frontages to Long Lane, Newcomen Street and Weston Street.
 - Improving Snowfields as a local neighbourhood shopping area. Upgrade footpaths and shop fronts along Snowfields and Newcomen Street to create a pedestrian-focused area.
- **Improve links and pedestrian environment**
 - Improving crossing points across Long Lane
 - Improving network of local routes through the area linking hospital with Long Lane. Re-establishing the traditional fine grain street pattern by creating new public access through large sites.
- **Improve open spaces**
 - Creating a shared surface linking St Georges Gardens with St George the Martyr Churchyard to improve landscaping and pedestrian priority and help create a quality link between London Bridge and Tabard Street.
 - Improving links to green spaces such as Leathermarket Gardens and Guy Street Park.
 - Providing good quality green spaces on estates, including spaces for food growing.

4.2.12 Detailed requirements for tall building applications

Applications for tall buildings will only be supported where they demonstrate they satisfy the following criteria:

1. Make a significant contribution to local regeneration

Tall building developments need to bring significant benefits for the local area and Southwark-wide. They will be expected to

make considerable contributions to public realm improvements, affordable housing, support for local businesses, job creation and training schemes, community facilities, open spaces, sports and recreation, and transport infrastructure over and above those normally expected of development (see section 6).

2. Tall buildings are located in the right place

Through a detailed urban design analysis, applications for tall buildings must demonstrate how they are an appropriate response to the surrounding context. This needs to consider natural topography, landscape features, the scale and character of surrounding buildings, urban grain and public realm, and important local views particularly at street level (see figure 24).

Tall building developments must be appropriate to the transport capacity of the area and will be expected to provide links to public transport and make improvements to transport infrastructure to overcome the impacts of a significant increase in density in the area.

Tall buildings must consider aviation constraints.

3. Outstanding architectural quality

Tall building developments must be of an exceptionally high standard and quality of architecture in terms of materials, form, massing, proportioning and silhouette.

Individually or as a group, they must form a distinctive and attractive landmark as viewed from all angles and contribute positively to the skyline and image of the Southwark and London.

Tall buildings will be expected to provide high quality internal environments and accommodation.

4. Sensitive to the historic context

Tall building developments must meet the requirements of the London View Management Framework regarding protection and enhancement of views and settings of strategically important landmarks.

Applications for tall buildings must demonstrate that they do not erode the value and importance of nearby conservation areas, the setting of listed buildings and structures, World Heritage Sites, scheduled ancient monuments, archaeological remains, historic parks and gardens or open spaces, including those in adjoining boroughs.

Tall buildings located in the setting of a World Heritage Site should not have a negative impact on the site's outstanding universal values as established in a World Heritage Management Plan and any additional local authority planning guidance.

5. Sustainable design and construction

Tall building developments must incorporate exemplary standards of sustainable construction and resource management, climate change mitigation and adaptation and potential for renewable energy generation and recycling. They will be expected to exceed the preferred standards set out in the Sustainable Design and Construction SPD.

6. Bring life and activity to an area and create vibrant and attractive streets and spaces

Tall building developments will be expected to bring significant enhancements to the local landscape and create a sense of place.

They will be expected to contain a mix of uses with public access, including significant amounts of leisure, cultural, arts

and entertainment space. They should incorporate publicly accessible areas on upper floors where appropriate.

Tall building developments will be expected to provide active uses, such as shops, restaurants and cafes along street level frontages in a way that relates positively to surrounding streets.

They will be expected to provide high quality public spaces, and deliver significant elements of the public realm strategy set out in this SPD.

7. Sensitive to its impact on the local environment

Applications for tall buildings must demonstrate they will not result in negative impact on the surrounding environment in terms of microclimate, wind turbulence, overshadowing, noise, reflected glare, night time appearance, traffic, aviation, navigation and telecommunication interference.

Applications for tall buildings must demonstrate they will not have a negative impact on the privacy and amenity on nearby residential accommodation.

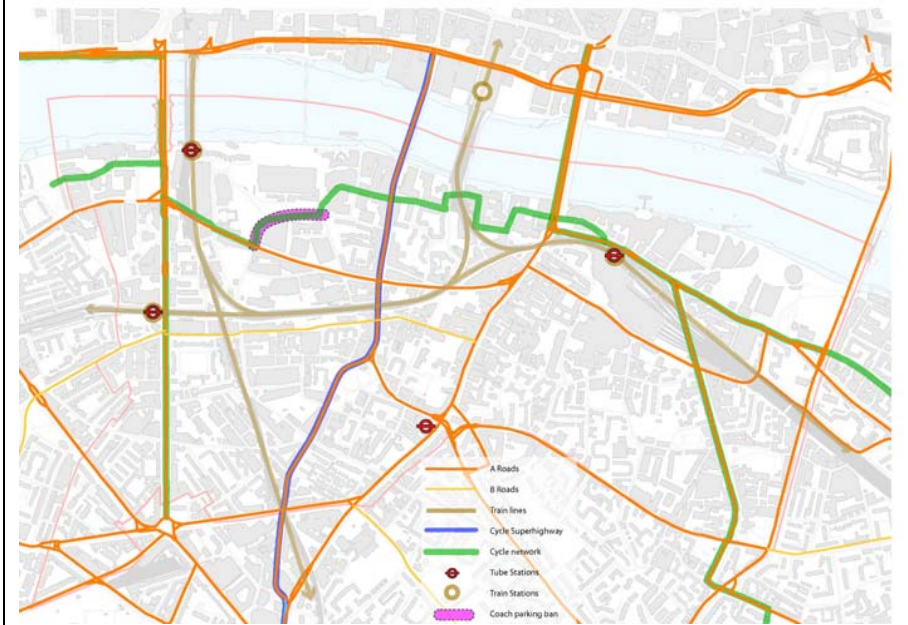
8. Application requirements

Applications for tall buildings must be accompanied by the following supporting material:

- Detailed Design and Access statement which explains the design approach to the proposal how it meets the above criteria. The statement should also demonstrate why a tall building is the best option for the site.
- A detailed views assessment which analyses and assesses the likely impact that a proposal would have on strategic views and important local views of relevance to the location of the proposal, as identified during discussions with the council.
- A three dimensional computer model of the proposal which can be used by the council to help assess the application.

4.3 Traffic and Transport

Figure 25: Traffic and transport network in Bankside, Borough and London Bridge



Bankside, Borough and London Bridge are busy places with major roads and transport infrastructure (see Figure 25). The River Thames also provides important opportunities for transport. Major improvements to transport infrastructure are planned or underway that are critical to supporting growth in the area. These include Thameslink Programme, and improved stations at Blackfriars and London Bridge.

New development will need to be designed so that it minimizes its impact on the road network and promotes walking, cycling and public transport. It is important to keep through-traffic off residential

as much as possible and to calm the movement of traffic in these areas.

We also need to make sure there are enough places for taxis, coaches and service vehicles to stop.

A high level of construction activity and road works can also cause disruption to roads and traffic movements through the area and it is important that this is carefully managed.

4.3.1 Guidelines for development

All development should:

- Be designed so that it does not contribute to traffic congestion and encourages walking and cycling and access to public transport.
- Exceed the requirements of Sustainable Transport SPD.
- Avoid the need for car parking, which will be restricted, with car-free development required in most cases.
- Provide on-site taxi, servicing and coach parking to meet the full demands of the development.
- Maximise cycle parking and exceed the standards set out in the Sustainable Transport SPD. Where public realm improvements are made, there should be an increase in on-street secure cycle parking.
- Maximise the use of the River Thames to transport construction waste and materials. We will protect existing transport facilities along the River for passengers and tourists.
- Contribute towards delivery of the traffic and transport improvement projects set out in section 4.5.2.

4.3.2 Planned and needed improvements to transport infrastructure to support growth

Section 4.3.4 set out key public realm projects needed in the opportunity area. Many of these will bring transport improvements such as improved cycle facilities and pedestrian links.

The table below sets out strategic transport projects that will help improve transport capacity and efficiency in Bankside, Borough and London Bridge.

Transport for London are undertaking detailed modeling of the transport infrastructure in the area and the likely impact that

increased development would have on this. The following list of projects will be updated once this modeling is complete.

Scheme	Key Benefits	Phasing
Jubilee Line upgrade	To provide additional capacity and improve journey times	2010
Thameslink Programme	Will significantly increase passenger capacity north-south to and through London through longer trains and more frequent services. The scheme will reduce overcrowding on the Underground. There will be a new station at Blackfriars with an entrance from the south bank. The Underground station will also be significantly enhanced There will also be new tracks and improvements to London Bridge Station.	2011-2015
Upgrade to the station and bus interchange at London Bridge.	A comprehensive redevelopment of London Bridge Station to reduce passenger congestion and interchange between modes of transport. Will create new entrances to the station and a new bus station and station concourse.	2011-2015
Northern Line upgrade	Phase 1: Provide additional capacity and improve journey times. Phase 2: Deliver a further 20 per cent increase in capacity through the simplification and recasting of service patterns.	2012 TBC
Crossrail	While not in Southwark it will provides benefits to the area by improving capacity on London's transport network and reducing congestion on the Underground.	2017
Improvements to buses	Bus capacity will increase by 40% between 2001 and 2011.	TBC
Central London cycle hire scheme	6000 bikes across 400 locations in zone 1. Will improve access to cycling.	2010

Scheme	Key Benefits	Phasing
Cycle superhighways	Will deliver cycle priority routes and improved facilities for cyclists. <ul style="list-style-type: none">- Merton to Bank- Woolwich to Tower Bridge- Penge to Bank	2010 after 2010 after 2010
River pier improvements	Creation of a new pier to be located between Festival and Bankside Piers and replacement of London Bridge Pier.	2012- 2015

4.4 Environmental considerations

New development needs to be sustainable and help tackle environmental issues such as climate change, flood risk and improving access to nature.

Bankside, Borough and London Bridge is within the River Thames flood zone, though is defended to a high level. There is a small risk that the defences may fail, making ground floor uses vulnerable. There are also large amounts of hard surfaces in the area and an old Victorian sewer system which also pose a flood risk.

It is also important that development does not damage the ecology or water quality of the River Thames.

As a very built up area there are less access to nature for people and wildlife.

Given the high density and the mix of uses in Bankside, Borough and London Bridge, there are good opportunities for creating and extending local energy networks. The LDA and Better Bankside are developing local energy network in the Bankside BID area. At London Bridge there is an opportunity to create a network based around Guy's Hospital and linking nearby development sites.

4.4.1 Guidelines for development

All development should:

- Avoid sensitive uses such as residential accommodation, schools and nurseries at ground and basement floor level, unless a site specific flood risk assessment demonstrates that flood risk poses a low hazard to the site.
- Be designed to be flood resilient, particularly basement and ground floor levels.
- Incorporate green roofs and habitat features.
- Incorporate sustainable urban drainage and reduce water run-off from the site by at least 50%. In this area this is likely to be achieved through green roofs, collection tanks and permeable landscaping of the public realm.
- Avoid polluting or damaging the ecology of the River Thames. Proposals to use water from the River to cool buildings must demonstrate that they will not negatively impact on the River quality or ecology, either individually or in combination with other similar schemes. A permit from the Environment Agency will be required.
- Explore opportunities to link into and develop local energy networks.
- Meet the preferred standards set out in the Sustainable Design and Construction SPD. In particular major development should achieve at least Code for Sustainable Homes Level 4 and BREEM Excellent.

5. Opportunity sites

A number of development opportunities have been identified in the area, based on development interest and appraisals of land use and built form. Figure 26 shows these sites.

A capacity and urban design analysis of these sites has allowed us to understand how we are likely to achieve our housing and employment targets for Bankside, Borough and London Bridge.

Figure 26: Opportunity sites in Bankside, Borough and London Bridge

A number of these sites are particularly important to achieving the vision of this SPD. For each of these we have set out more detailed guidance on land uses, design and contribution to public realm enhancements. This should be read together with the more general guidance in section 4.

5.1 London Bridge Station

Why is this site important?

- London Bridge Station is a major arrival and transport interchange point. It is one of the busiest stations in the country, with 115 million people passing through in 2008. The station is at capacity and needs upgrading to cope with growth in passengers.
- Upgrading and improving the station and interchange at London Bridge is essential to unlocking the area's potential.

- Giving the station a high profile image will mark its importance and contribute to the changing reputation of London Bridge as an important part of central London.
- Redevelopment of the station provides a significant opportunity to address a number of key public realm issues to improve links south of the viaduct and enliven St Thomas Street.
- The position of access points will have a major impact on the movement through surrounding area.

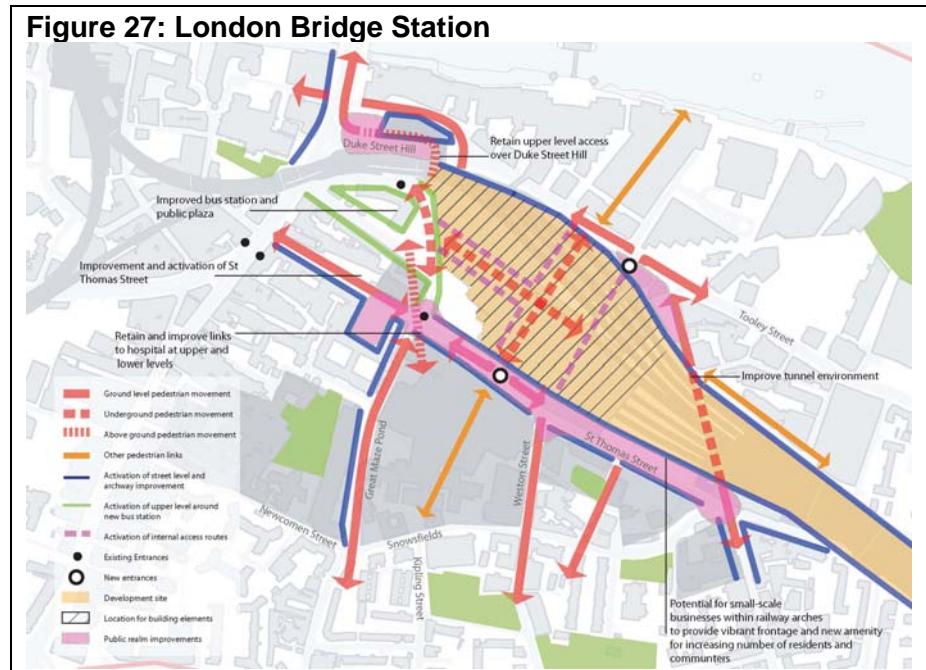
What issues need to be addressed?

- Currently there is a very confusing movement pattern out of the station and within the station between transport modes.
- Platforms and concourses are congested in peak times.
- The station structure itself inhibits movement through the surrounding area.
- Redevelopment and the location of access points and public spaces need to relate to key destination points from the station. These are:
 - north across London Bridge Station to the City. This is one of the busiest routes out of the station. By 2011 this route is projected to be used by 21,000 people an hour at peak times.
 - south to the Guy's Hospital and King's College Campus. 20,000 are expected to visit this site by 2020.
 - east to commercial areas and visitor attractions along Tooley Street and the Riverside.
 - west to Borough Markets, Clinks Street, Bankside and Borough High Street.

- the link south to St Thomas Street and Bermondsey Street will become increasingly important following major development in the area.
- Redevelopment needs to respond to the changes in levels around the site and the different opportunities for access these provide.
- Existing narrow footpaths particularly along Tooley Street make it congested place to walk during rush hour.
- There is a need to redesign and upgrade the bus station at London Bridge Station to help it better connect with rail services and cope with an increased flow of buses and taxis and be a more pleasant environment. The station also acts as a barrier to movement and creates a poor visual link with the train station interchange to the rear.
- London Bridge station is a major trip generator for taxis; however, the drop-off and pick-up facilities are poor and limited to the concourse entrance.
- Planned Thameslink works need to be accommodated, including a track layout, increased number of through platforms, larger and more frequent trains.
- Much of the historic fabric of the station is listed, such as the railway arches along St Thomas Street.
- The railway arches and viaduct do not animate the street.
- Network Rail is revisiting the currently approved station redevelopment scheme because of the complexity of upgrading the station within a fixed budget and programme.

How should the site be developed?

Figure 27 illustrates the redevelopment principles for London Bridge Station.



Land Use

- Achieving a modern and well functioning station and transport interchange should be the primary focus of any redevelopment scheme.
- The station could also become a major destination in its own right offering a mix of commercial uses both within the arches and in an air-rights development over the platforms.
- Retail (A class) floor space should be increased, providing a range of shop units to improve the mix in the area. This should include a mid-sized supermarket, as in the consented scheme.
- Existing D class uses should be reprovided and preferably increased.
- The site is also suitable for a significant amount of office (B class) floor space.

Movement

- Redevelopment should achieve a spacious, uncluttered and legible station that provides easy interchange between transport modes and is able to accommodate increased passenger flows.
- The design needs to consider the combined passenger movement from all transport modes within the station.
- Redevelopment needs to improve movement out of the station. The station should open up onto street level, providing clear and legible new access points onto Tooley Street and St Thomas Street.
- New access points should be provided near the centre of the station, east of the existing access points.
- All entrances should be linked internally forming a permeable internal street network, helping to knit the station into the surrounding street system.
- Opportunities for reconfiguring the flow of traffic and phasing of the lights on Tooley Street outside the station are limited. As such a high level link across Tooley Street linking the station with London Bridge should be integrated into the design of the development to efficient movement of pedestrians along this major link.
- A high level link from the station to Guy's Hospital should also be provided.
- Redevelopment scheme should provide convenient cycle parking and storage facilities.

Built Form

- Given the constraints of the site and substantial public benefit of an improved station, some loss of the historic fabric may be considered. The redevelopment scheme must demonstrate how it has minimized the loss of historic fabric.
- Any intervention into the station fabric must be of exceptional architectural quality creating a landmark development.

- The site lies close to Bermondsey Street and Borough High Street Conservation Areas. Development must not have a negative impact on the character of these conservation areas.
- The site is identified as having potential for a tall building element, subject to the guidance set out in sections 4.3.3 and 4.3.13.
- An air-rights building above the station platforms should be a distinguished building in its own right, of high architectural quality.

Public Realm

- Redevelopment of the station will need to provide an attractive environment for everyone using the station.
- Key to accommodating large volumes of people will be improving and increasing public space and pedestrian priority around the station, particularly at station entrances.
- A key element of the redevelopment will be the provision of a public plaza/bus and taxi interchange opening up the upper level entry to the station and integrating with the Shard and London Bridge Place developments.
- Activity within the station should be integrated with life on the surrounding streets. This includes through opening up the station onto Tooley Street and St Thomas Street and providing active frontages along the internal streets/ concourse.
- The railway arches onto St Thomas Street should be refurbished and brought into active use to help revitalize the street.
- Active uses should also be provided at the corner of Bermondsey Street and Tooley Street.

5.2 Guy's Hospital and King's College**Why is this site important?**

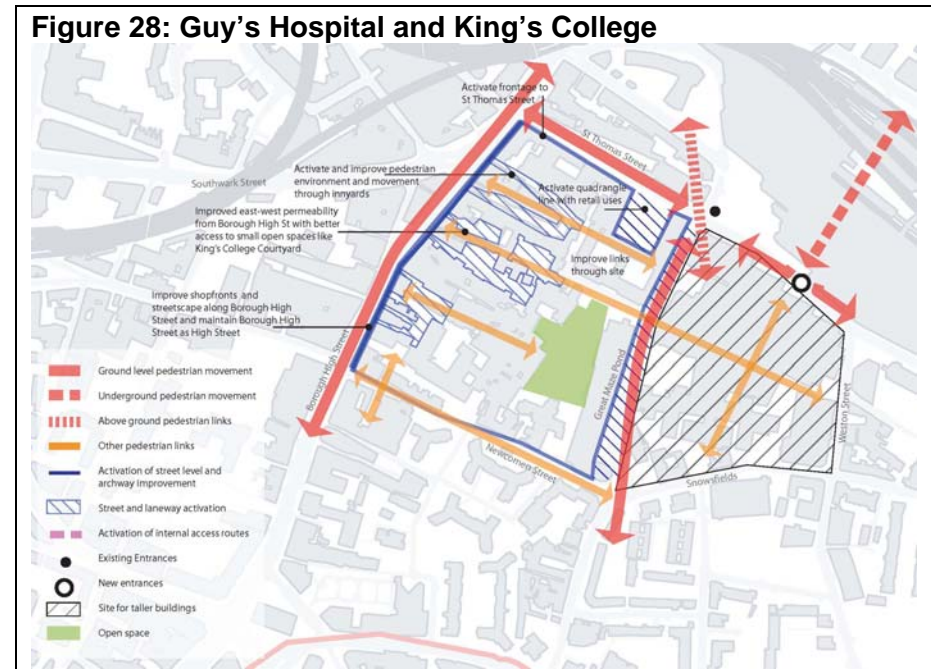
- Guy’s Hospital and King’s College together are a major institution and destination in Southwark, visited by over 15,000 people each day. It is a major teaching hospital with a reputation for clinical research excellence.
- It also provides a range of important health services to London and the local community.
- The hospital and college occupy a very large site in the heart of London Bridge and provide important links through from the station and Borough High Street, in particular Great Maze Pond and a number of coaching-inn yards off Borough High Street.
- The hospital trust is preparing a master plan for the site to enhance and integrate hospital services and research facilities, a new state-of-the-art cancer centre.
- Redevelopment of the site provides an opportunity to better integrate the site with the surrounding area, improve links and permeability and the mix of uses and activity.

What issues need to be addressed?

- The campus occupies a large site in London Bridge, yet is inward looking and does not integrate well with the surrounding area.
- Poor east-west permeability.
- The internal environment of the site could be improved to provide a better quality public realm, activate key routes and improve way finding.
- Green spaces in the site have the potential to be better linked to the surrounding area to become community assets.
- Part of the site is within the Borough High Street Conservation Area. The site contains a number of buildings historic and architectural value, as well as buildings which detract from the area, such as Guy’s Tower.

How should the site be developed?

Figure 28 illustrates the redevelopment principles for the site.



Land Use

- Achieving a world-class health, research and education facility for all of London and improved health services for the local community should be the primary focus of any redevelopment scheme.
- This can be supported by additional development for a range of uses to create a vibrant and mixed use quarter.
- A range of uses should be provided, including housing, student accommodation, offices, retail, leisure, entertainment, arts and cultural uses.
- A and D Class uses should be made accessible by the general public.

- Existing facilities such as Greenwood Theatre should be retained or reprovided.

Movement

- Redevelopment should improve permeability and pedestrian movement east-west and north-south through the site.
- This should include opening up a new public access linking Snowsfields with the new entrance to London Bridge Station.
- Public access into the hospital should be improved by creating a new entrance onto Tooley Street.
- East-west links should be improved linking Borough High Street with Weston Street, improving pedestrian activity through the coaching inn-yards.
- High level access over St Thomas Street gives quick access from the bus and rail station and should be retained.
- Snowsfields/Newcomen Street should remain the primary means of vehicular access into the site.
- Redevelopment should improve wayfinding through the hospital, adopting the principles of the Legible London project.
- A car parking strategy should be prepared for future hospital development to ensure an appropriate provision of car parking and a reduction in surface level car parking.

Built Form

- Development must not negatively impact on the character of the Borough High Street Conservation Area and should improve the setting of historic buildings and buildings of townscape value.
- Development schemes should improve the appearance of the Guy's Tower.
- Part of the site east of Great Maze Pond is identified as having potential for a tall building element, subject to the guidance set out in sections 4.3.3 and 4.3.13.
- Higher building forms offer the opportunity to consolidate hospital functions, freeing up sites for development to improve the mix of uses and activity. This should also enable the provision of improved public realm.

- The scale of development should step down along the boundaries to integrate with the scale of development in Bermondsey Village, Borough High Street and Tabard Gardens.

Public Realm

- The redevelopment of the hospital should include the creation of a more prominent, attractive and welcoming entrance for the hospital onto St Thomas Street.
- Frontage along St Thomas Street should be set back opposite the new entrances to London Bridge Station to assist with the movement of large volumes of people.
- Active frontages should be provided along St Thomas Street to help revitalize the street. This should open up Boland House forecourt to the street as a new public space surrounded by active uses such as cafes and restaurants.
- Development fronting Newcomen/Snowsfields should help define and bring activity to the street so that it does not act as a 'back entrance'.
- Redevelopment should provide a high quality public realm throughout the hospital site.
- New public spaces should be created and linked with existing spaces. This includes a continuation of the green links connecting open spaces in Bermondsey Village.
- Routes through the site should be improved to give greater priority to pedestrians. Key routes should be defined by active frontages.
- Redevelopment should help bring improvements to the shopfronts and coaching-inn yards along Borough High Street.

5.3 Colechurch House

Why is this site important?

- This site is located on a prominent corner at the gateway into London Bridge.

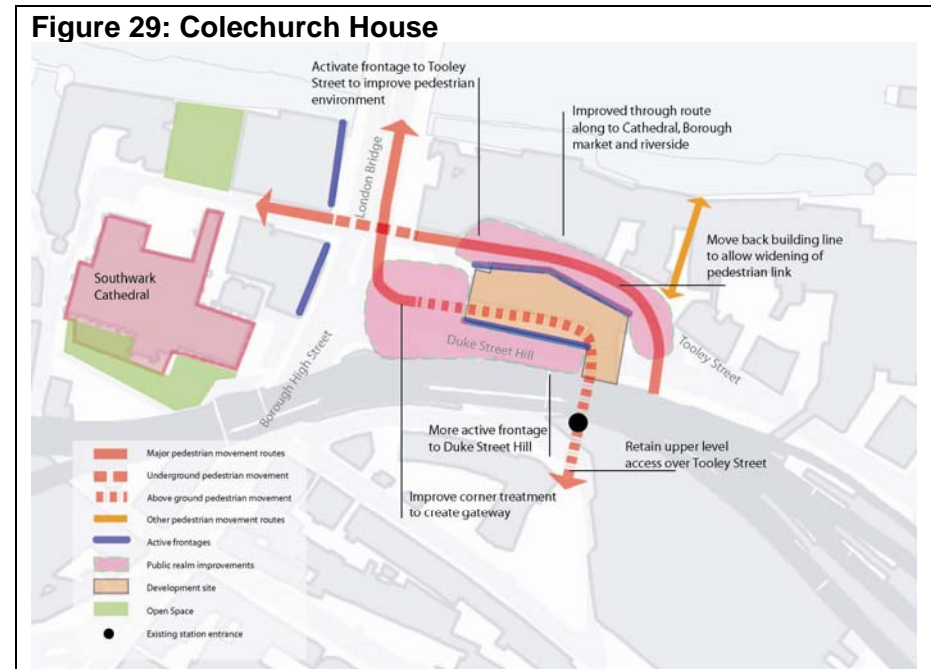
- The site is key to providing strong pedestrian links between London Bridge Station and London Bridge and into the Borough Market/Southwark Cathedral precinct.
- Development can make a significant contribution to improving the supply of high quality office accommodation.

What issues need to be addressed?

- The link from London Bridge Station across London Bridge to the City is a key route.
- The London Bridge underpass allows access into the Borough Market/Southwark Cathedral precinct without having to cross London Bridge or Borough High Street. However the route is not well defined or pedestrian-friendly and has a narrow paving along Tooley Street.
- Much of the street level frontages of the existing building are inactive.
- Redevelopment needs to respond to the changes in levels around the site and the different opportunities for access these provide.
- This site is located within the Thames Policy Area and next to Borough High Street and Tooley Street Conservation Areas.
- The design of development in this prominent location will be very important.

How should the site be developed?

Figure 29 illustrates the redevelopment principles for the site.



Land Use

- Redevelopment of the site must reprovide and preferably increase the B class floor space.
- Additional floor space could be offices, retail (A class) or leisure (D class) uses.

Movement

- Development of the site should facilitate the flow of pedestrians from London Bridge Station across London Bridge. A high level link across Tooley Street linking to the station should be provided and integrated into the design of the development.
- Development should improve the pedestrian link along Tooley Street.

Built Form

- Development will need to be sensitive to its riverside location and take into account important views.
- This is not a location suitable for a tall building. Development must respect established heights of buildings along the river.
- The building line should be set back from the Tooley Street frontage to allow widening of the footpath. Reducing the setback along Duke Street Hill must not be at the expense efficient pedestrian movement.

Public Realm

- Development should provide active frontages to Duke Street Hill, Tooley Street and London Bridge.
- Development should improve the gateway treatment of the corner of Duke Street Hill and London Bridge.
- The treatment of the public realm along Duke Street Hill should assist with the flow of people from the station entrance.

5.4 Fielden House

Why is this site important?

- The site is next to the Shard development and fronts onto the planned new public plaza and bus station.
- The site is key in improving access from the upper level of the station onto St Thomas Street.
- The site is also important to improving St Thomas Street and the Joiner Street entrance to the station.
- Development can make a significant contribution to improving the supply of high quality office accommodation.

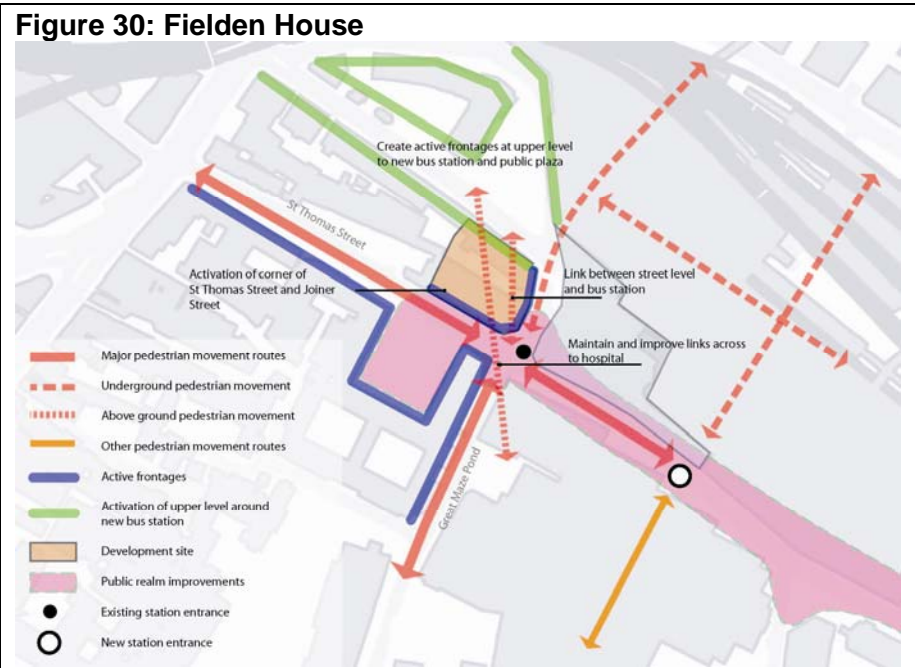
What issues need to be addressed?

- Redevelopment needs to respond to the changes in levels around the site and the different opportunities for access these provide.

- The site lies opposite the listed Boland House and is within the Borough High Street Conservation Area.

How should the site be developed?

Figure 30 illustrates the redevelopment principles for the site.



Land Use

- Redevelopment of the site must reprove and preferably increase the B class floor space.
- Additional floor space could be offices, retail (A class) or leisure (D class) uses.

Movement

- Development should improve access from the station plaza onto St Thomas Street.
- Development should reinforce the Joiner Street entrance into the station and the Great Maze Pond entrance into the hospital.
- Access into the site should be provided from St Thomas Street and London Bridge Street.

Built Form

- The site should have a strong corner presence.
- This is not a location suitable for a tall building. The development should be designed so that building elements match the established heights of buildings along both St Thomas Street and London Bridge Street.
- Building elements should follow the established building line and provide enclosure to both St Thomas Street and London Bridge Street.

Public Realm

- Active frontages should be provided at lower level along St Thomas Street and Joiner Street, and at upper level along London Bridge Street.

5.5 St Thomas Street car park and adjoining sites

Why is this site important?

- This is a gap site located at the important junction of St Thomas Street, Bermondsey Street and Snowsfields.
- Redevelopment of the site could make a significant contribution to revitalising St Thomas Street and creating a destination to draw people south through the viaducts.
- The site is close to planned new access points to London Bridge Station.
- Development on the site could make a significant contribution to providing high quality housing in London Bridge.

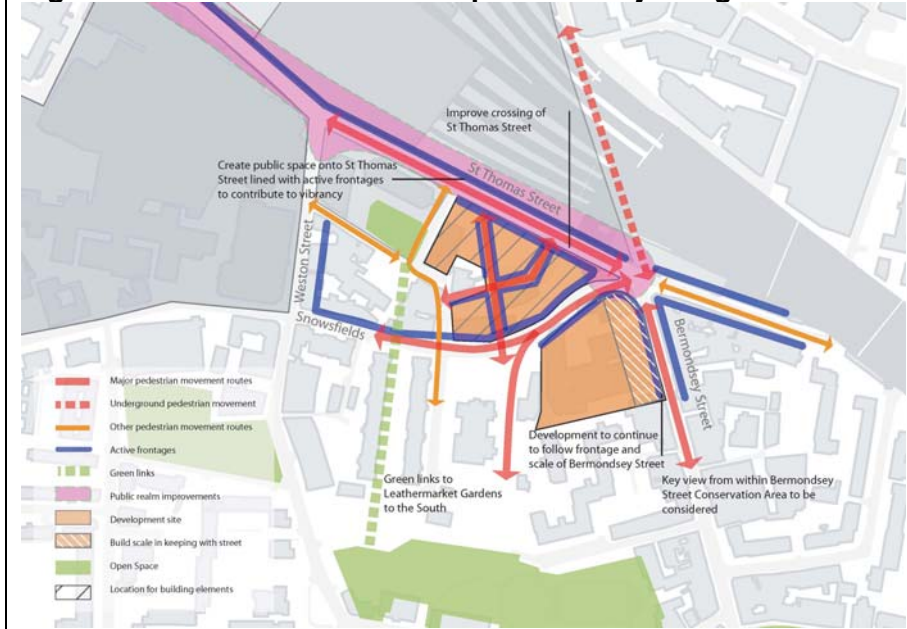
- The site is identified as having the potential for tall buildings.

What issues need to be addressed?

- The site lies within the Bermondsey Conservation Area and is close to a number of Grade II and II* listed buildings.
- The site lies within the background assessment area of strategic views of St Paul's. Views from within the Tower of London and up Bermondsey Street are also important considerations.
- Snowsfields is an important route south from St Thomas Street and links with a number of streets that provide links into Bermondsey Village.

How should the site be developed?

Figure 31 illustrates the redevelopment principles for the site.

Figure 31: St Thomas Street car park and adjoining sites

Land Use

- The site is appropriate for residential uses.
- A mix of other uses should be provided. In order for development to create a new destination to draw people south of the viaduct and revitalize St Thomas Street, a substantial proportion of active uses should be incorporated into the development. These would include retail (A class), community, leisure and entertainment (D class) uses.
- This should include the provision of a major leisure, arts, cultural or entertainment facility to provide public benefit and create a significant amount of footfall.
- Existing B class floor space should be reprovided on the site, preferably as small business units.

Movement

- The form and layout of development should improve links through the site between St Thomas Street and Bermondsey Village. In particular improve links down Snowfields and to Leathermarket Gardens.

Built Form

- Development must not negatively impact on the character and townscape of the Bermondsey Street Conservation Area.
- The form and layout of the development should reflect the historic grain of the conservation area.
- The site is identified as having potential for a tall building element, subject to the guidance set out in sections 4.3.3 and 4.3.13.
- The established view of the line of buildings from Bermondsey Street looking north should not be dominated by the form of the new development.
- Building heights should step down to the Bermondsey Street corner. The frontage to St Thomas Street should continue the established heights and building line of the street.
- Residential accommodation will be expected to be of the highest standard and provide a good internal living environment.
- The form of the building must be appropriate to accommodate substantial amounts of publicly accessible active uses.

Public Realm

- Development will be expected to provide pleasant and welcoming public spaces at street level. These should be lined with active uses.
- Development should be set back from St Thomas Street to improve the pedestrian environment. Public space should also be provided near the corner with Bermondsey Street.
- Active uses and publicly accessible levels within the development should at least match the height of the viaduct to assist in creating a major destination point.

- Development should help improve pedestrian crossing of St Thomas Street.

5.6 Capital House and Beckett House, Weston Street

Why is this site important?

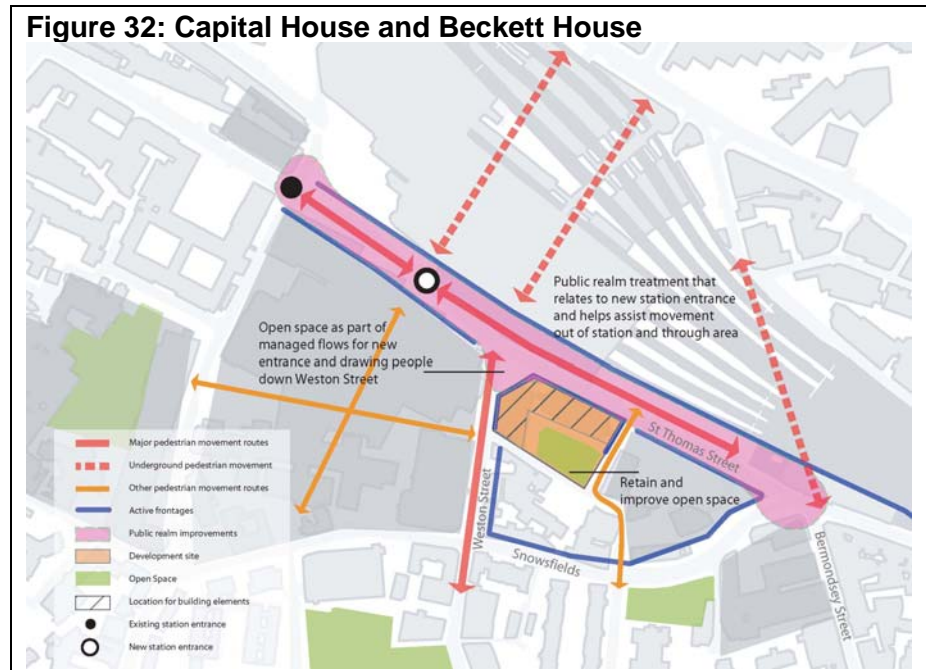
- The site is in a key location opposite the planned new entrance to London Bridge Station and opposite Guy's Hospital and King's College campus.
- The site is identified as having the potential for tall buildings.
- Redevelopment of the site could make a significant contribution to revitalising St Thomas Street.
- Development on the site could make a significant contribution to providing high quality housing and office space in London Bridge.

What issues need to be addressed?

- The site borders on the Bermondsey Conservation Area and is close to a number of Grade II and II* listed buildings. Proposals and develop should preserve or enhance the existing character.
- The site lies within the background assessment area of strategic views of St Paul's. Views from within the Tower of London are also important consideration.
- The site contains areas of open space which should be preserved and enhanced.
- Making the most of links from the new station entrance.

How should the site be developed?

Figure 32 illustrates the redevelopment principles for the site.



Land Use

- Redevelopment of the site must reprovide and preferably increase the B class floor space.
- Additional floor space could be offices, retail (A class), leisure, education and community (D class) uses or residential accommodation.

Movement

- Development of the site should help improve links from the new station entrances along Weston Street and St Thomas Street.

Built Form

- Development must not negatively impact on the character and townscape of the Bermondsey Street Conservation Area.

- The site is identified as having potential for a tall building element, subject to the guidance set out in sections 4.3.3 and 4.3.13.
- Development should be sensitive to the smaller scale of properties to the south of the site.
- Residential accommodation will be expected to be of the highest standard and provide a good internal living environment.

Public Realm

- The building line should be set back from St Thomas Street to improve the pedestrian environment. A public space should be provided at the corner of St Thomas Street and Weston Street to accommodate pedestrian flows from the new station entrance.
- Active frontages should be provided to St Thomas Street and Weston Street.
- Development should help improve crossing of St Thomas Street.
- The development should retain and improve the open space to the rear of Beckett House and open up public access to this space.
- Good quality pedestrian routes are encouraged through the site, increasing permeability from St. Thomas and Weston Street.

5.7 144 to 152 Bermondsey Street

Why is this site important?

- It is a poor quality building within a conservation area.
- Development of the site will help continue the successful rejuvenation of Bermondsey Street by providing further activation at the southern end of the street and helping to reinstate the historic urban grain.
- Development of the site will help provide new residential accommodation in London Bridge.

What issues need to be addressed?

- The scale and character of the Bermondsey Street Conservation Area.
- Permeability and public access through the site.
- Improving the Bermondsey Street frontage.

How should the site be developed?

Figure 30 illustrates the redevelopment principles for the site.



Land Use

- Redevelopment of the site must reprovide the B class floor space.

- Additional floor space could be B class uses, retail (A class), leisure, community (D class) uses or residential accommodation.

Built Form

- Development should reinstate a more traditional built form, following the layout of the existing street pattern and reinforcing the existing street edge.
- Building elements should not dominate the Bermondsey Street frontage and should relate to the streetscape on all elevations.
- Building heights to relate to those of surrounding buildings, being sensitive to the historic character of the area. Higher building elements should be setback from frontages.

Public Realm

- Development should respond to the historic fine grain of Bermondsey Village and create pedestrian links through the site.
- Public space should be provided in the site, lined with active uses.
- Active frontages should be provided to Bermondsey Street.

5.8 180 Borough High Street

Why is this site important?

- This site is located above archaeological remains of national significance which are likely to be included in the Schedule of Ancient Monuments.
- The site has a prominent corner location at the junction of Borough High Street and Marshalsea Road opposite tube station.
- The site has an important relationship to St. George the Martyr Church on the opposite side of Borough High Street.
- The site is located next to Little Dorrit Park

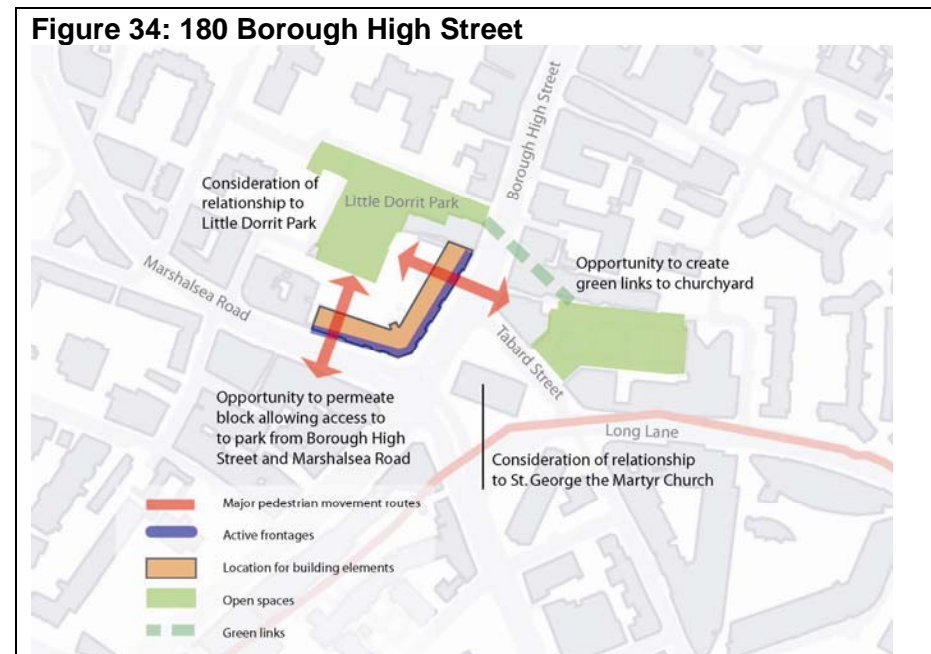
- The site provides office accommodation in Borough.

What issues need to be addressed?

- The existing block is impermeable and does not provide active frontage along Borough High Street or Marshalsea Road.
- Any development will require an archaeological evaluation and the identification and preservation of archaeological remains in situ, in line with policy and national guidance.
- The site is close to Borough High Street Conservation Area and proposals should be mindful of existing scale, grain and character of the High Street.

How should the site be developed?

Figure 34 illustrates the redevelopment principles for the site.



Land Use

- Redevelopment of the site must reprovide the B class floor space.
- Additional floor space could be B class uses, retail (A class), leisure, community (D class) uses or residential accommodation.

Movement

- Development should improve links between Little Dorrit Park and St. George the Martyr Churchyard.

Built Form

- Development should follow the historic building line of Borough High Street and Marshalsea Road and reinforce the corner.
- Development should be of high quality architecture appropriate to its corner setting.
- Breaking up the perimeter block is encouraged to increase permeability and reinstate the historic street pattern through the extension of Disney Place.
- The parapet height of the main body of St. George the Martyr church establishes a prevailing height in the immediate area with its spire dominating local views. The development should respect this height and maintain local views of the spire from Little Dorrit Park.
- The rear of the building is as important as the front as views from Little Dorrit Park of the development must be considered. Appropriate treatment should be given to materials and views in this regard.
- Construction of basements on site is not advised without an archaeological evaluation, as designs of this nature may not be able to preserve archaeological remains in situ. Existing foundations should be tested for reuse or their locations drilled for new piles to ensure impacts upon buried archaeological remains are minimised.

Public Realm

- Better links should be made to Little Dorrit Park from Borough High Street and Marshalsea Road.
- Development of the site should provide open space to the rear of the development opening out onto the park.
- Active frontages should be provided to Borough High Street and Marshalsea Road.

5.9 1 Blackfriars Road**Why is this site important?**

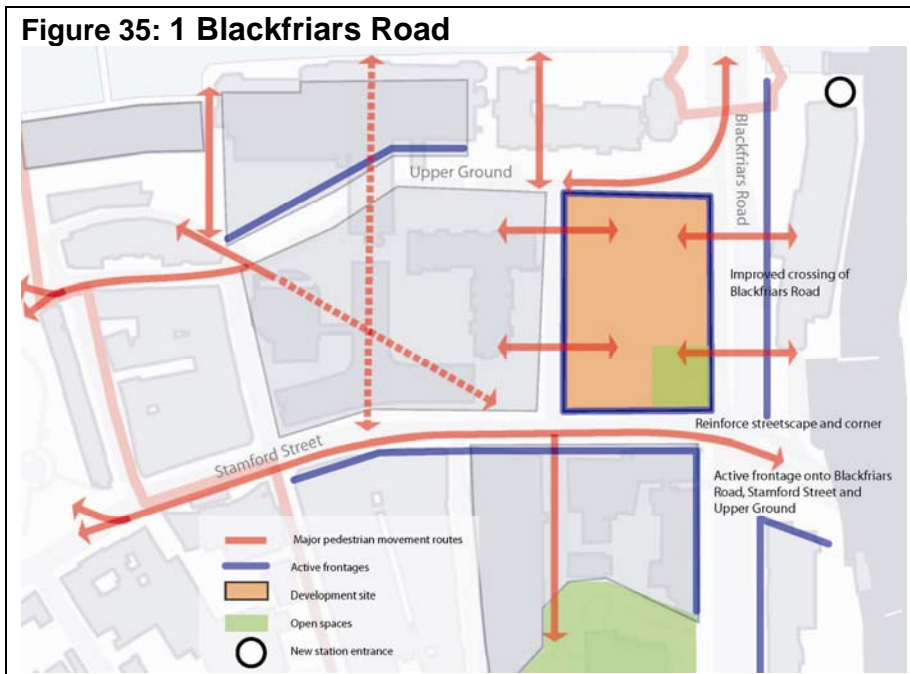
- This is a large vacant site located at a major intersection of Stamford Street, Blackfriars Road and Southwark Street at a gateway to Southwark.
- Redevelopment of the site could make a significant contribution to improving the streetscape and vibrancy of Blackfriars Road.
- The site is located close to the planned new entrance to Blackfriars station.
- Development on the site could make a significant contribution to providing high quality housing and offices Bankside.
- The site is identified as having the potential for tall buildings.

What issues need to be addressed?

- The design of development in this prominent location will be very important.
- The site is located in the Thames Policy Area.
- The impact of development within the view from St James Park to Horse Guards Road and along Blackfriars Road will be important.

How should the site be developed?

Figure 35 illustrates the redevelopment principles for the site.



Land Use

- The development should provide an appropriate mix of hotel, office (B class), retail (A class), leisure, entertainment and cultural (D class) uses with active uses at several of the lower levels.
- This should include the provision of viewing platforms and a major leisure, arts, cultural or entertainment facility to provide public benefit and take advantage of its prominent location.
- Residential uses should also be provided.

Movement

- Development should provide new pedestrian links through the site which relate and link to nearby open spaces, including those on the adjoining Kings Reach and 20 Blackfriars Road sites.

- Development should help improve the pedestrian and cycling environment on Blackfriars Road and Stamford Street including through provision of improved road crossing.

Built Form

- Development will need to be sensitive to its riverside location and take into account important views.
- The site is identified as having potential for a tall building element, subject to the guidance set out in sections 4.3.4 and 4.3.13.
- Development should provide definition to the corner.
- The building line along Blackfriars Road should provide strong enclosure to the street and a continuation of the established building line of the street.
- Along Stamford Street the building line should help enclose the street.
- Residential accommodation will be expected to be of the highest standard and provide a good internal living environment.
- The form of the building must be appropriate to accommodate substantial amounts of publicly accessible active uses.

Public Realm

- Development should provide pleasant and welcoming public space on the site which relate and link to nearby open spaces, including those on the adjoining Kings Reach and 20 Blackfriars Road sites.
- Development should help improve the streetscape of Blackfriars Road and Stamford Street.
- Active frontages should be provided along all street elevations and onto public spaces.

5.10 20 Blackfriars Road

Why is this site important?

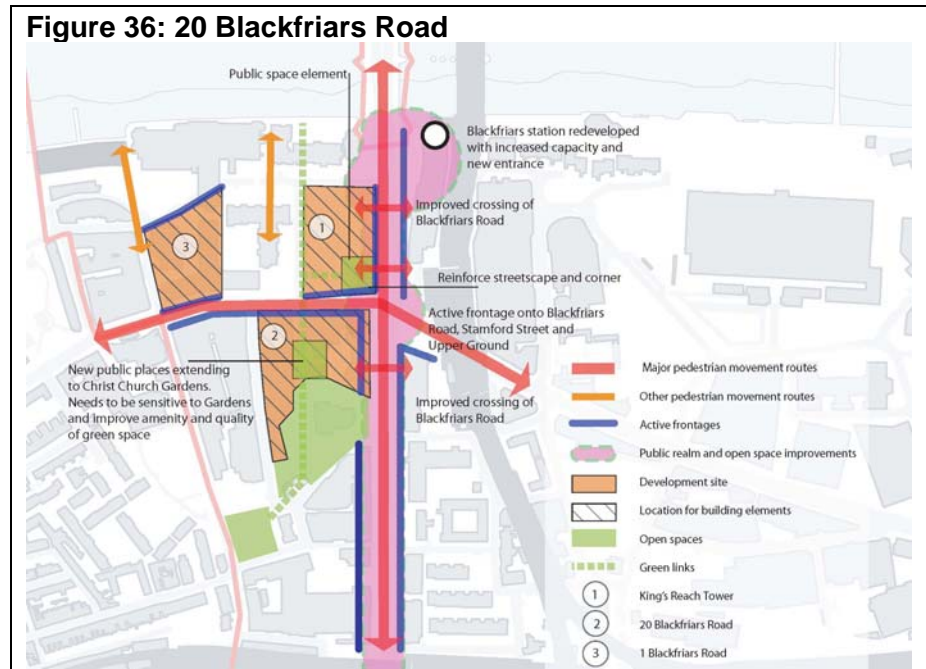
- This is a large site located at a major intersection of Stamford Street, Blackfriars Road and Southwark Street at a gateway to Southwark.
- Redevelopment of the site could make a significant contribution to improving the streetscape and vibrancy of Blackfriars Road.
- The site is close to the planned new entrance to Blackfriars station.
- Development on the site could make a significant contribution to providing high quality housing and offices Bankside.
- The site is identified as having the potential for tall buildings.

What issues need to be addressed?

- The listed building at 1-3 Stamford Street is located within the site.
- The site is located next to Christ Church Gardens and its relationship with the open space needs to be well considered.
- The design of development in this prominent location will be very important.
- The impact of development within the view from St James Park to Horse Guards Road and along Blackfriars Road will be important.

How should the site be developed?

Figure 36 illustrates the redevelopment principles for the site.



Land Use

- Redevelopment of the site must reprovide and preferably increase the B class floor space. Small business units should be provided.
- Additional floor space should be an appropriate mix of office (B class), retail (A class), leisure, entertainment and cultural (D class) uses with active uses at several of the lower levels.
- This should include the provision of a major leisure, arts, cultural or entertainment facility to provide public benefit and take advantage of its prominent location.
- Residential uses should also be provided.

Movement

- Development should provide new pedestrian links through the site which relate and link to nearby open spaces, including Christ Church Gardens and those on the adjoining 1 Blackfriars Road site.
- Development should help improve the pedestrian and cycling environment on Blackfriars Road and Stamford Street including through the provision of improved road crossings.

Built Form

- The development must retain and be sensitive to the setting of 1-3 Stamford Street.
- The site is identified as having potential for a tall building element, subject to the guidance set out in sections 4.3.4 and 4.3.13.
- Development should provide definition to the corner.
- The building line along Blackfriars Road should provide strong enclosure to the street and a continuation of the established building line of the street.
- Along Stamford Street the building line should help enclose the street.
- The form and layout of the development will be expected to relate well to Christ Church Gardens.
- Residential accommodation will be expected to be of the highest standard and provide a good internal living environment.
- The form of the building must be appropriate to accommodate substantial amounts of publicly accessible active uses.

Public Realm

- Development should provide pleasant and welcoming public space on the site which links to Christ Church Gardens. Building elements should not cause overshadowing or negative impacts on
- Development should help improve the streetscape of Blackfriars Road, Stamford Street and Paris Gardens.

- Active frontages should be provided along all street elevations and onto public spaces.

5.11 King's Reach Tower**Why is this site important?**

- This is a large site located close to the River a gateway to Southwark.
- This site offers the opportunity to enhance shopping provision in the area.
- Redevelopment of the site could help improve links through to the Riverside and nearby open spaces.
- The site is located in close proximity to the planned new entrance to Blackfriars station.
- Development on the site could make a significant contribution to providing high quality housing and offices in Bankside.
- The site is identified as having the potential for tall buildings.

What issues need to be addressed?

- The design of development in this prominent location will be very important.
- The site is located in the Thames Policy Area.
- The site contains a protected shopping frontage.
- The impact of development within the view from St James Park to Horse Guards Road and along Blackfriars Road will be important. Views from County Hall from western end of Westminster Bridge and Royal Festival Hall from Golden Jubilee Bridges should also be considered.
- The relationship of development to Oxo Tower which is a landmark in the area needs to be considered.
- Impact of development on the amenity and servicing arrangement of Rennie Court needs to be considered.

How should the site be developed?

Figure 37 illustrates the redevelopment principles for the site.



Land Use

- Redevelopment of the site must reprovide and should increase the B class and A class floor space.
- Small business units should be provided.
- Additional floor space should be an appropriate mix of office (B class), retail (A class), leisure, entertainment and cultural (D class) uses with active uses at lower levels.
- Residential uses could also be provided. Improvements to the residential accommodation in Rennie Court are also encouraged.

Movement

- There should be improved pedestrian links through the site improving access between Stamford Street, Upper Ground, Barge House Street and to the Riverside. Links should relate to those to provided on adjoining sites.

Built Form

- Development will need to be sensitive to its riverside location and take into account important views.
- The site is identified as having potential for a tall building element, subject to the guidance set out in sections 4.3.4 and 4.3.13.
- The form and arrangement of buildings elements should help break down the scale of the main tower, improve permeability of the site and better respond to the height, scale and massing of neighbouring buildings. This could be a clustering of smaller building elements around the tower.
- Building form should help provide definition and enclosure to street frontages.
- Residential accommodation will be expected to be of the highest standard and provide a good internal living environment.

Public Realm

- Development should provide pleasant and welcoming public space on the site which relate and link to nearby open spaces.
- Development should help improve the streetscape of Blackfriars Road and Stamford Street.
- Active frontages should be provided along all street elevations and onto public spaces. These should include a high proportion of retail uses.
- Development should improve the amenity space for occupants of Rennie Court.

5.12 SEA CONTAINERS HOUSE

Why is this site important?

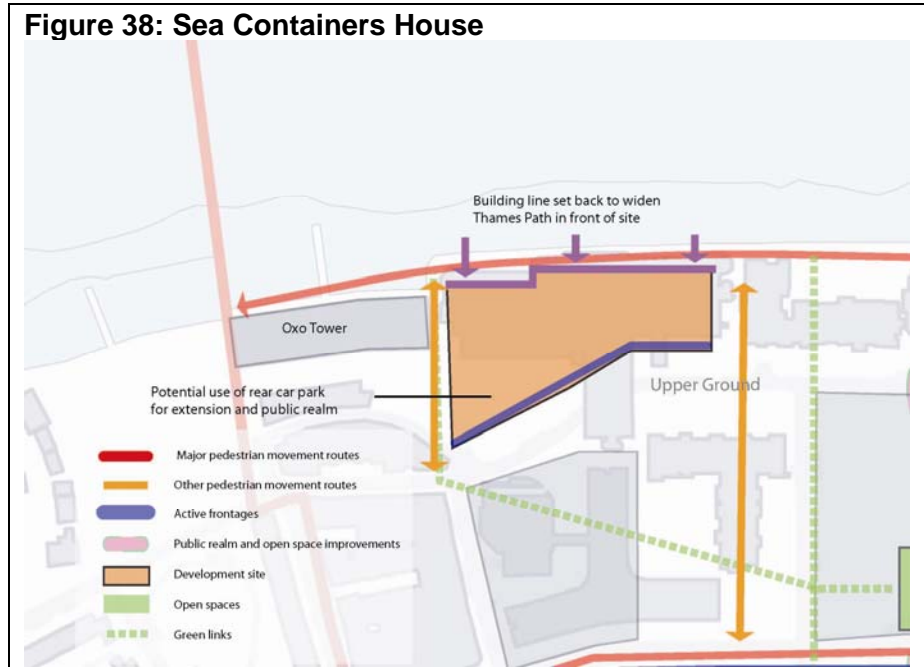
- This is a large site located on the riverfront at a gateway to Southwark on an important route between the South Bank Centre and Tate Modern.
- Redevelopment of the site could help improve links through to and along the Riverside.
- The site is located in close proximity to the planned new entrance to Blackfriars station.
- Development on the site could make a significant contribution to providing high quality housing and offices in Bankside.

What issues need to be addressed?

- The design of development in this prominent location will be very important.
- The site is located in the Thames Policy Area.
- The impact of development within the view from St James Park to Horse Guards Road and along Blackfriars Road will be important. Views from County Hall from western end of Westminster Bridge and Royal Festival Hall from Golden Jubilee Bridges should also be considered.
- The relationship of development to Oxo Tower which is a landmark in the area needs to be considered.
- The riverside walk suffers from a bottleneck in this location.

How should the site be developed?

Figure 38 illustrates the redevelopment principles for the site.



Land Use

- Redevelopment of the site must reprovide and should increase the B class floor space.
- Additional floor space should be an appropriate mix of office (B class), retail (A class), leisure, entertainment and cultural (D class) uses with active uses at lower levels.
- Residential uses could also be provided.

Movement

- Development should provide improved access to the riverfront and improvements to the riverside walk.
- Links should relate to those provided on King's Reach and 1 Blackfriars Road sites.

Built Form

- This site is not appropriate for tall buildings.
- Development will need to be of exceptional architectural quality and relate to its riverside location and take into account important views.
- The building line should be set back further from the Riverside Walk to enable it to be widened.
- Building heights should scale down along the river frontage. The scale and height of development should not challenge the height and form of the Oxo Tower.
- Residential accommodation will be expected to be of the highest standard and provide a good internal living environment.

Public Realm

- The development should enable the widening and improvement of the riverside walk to .
- Active frontages should be provided along the riverside walk.

5.13 CROSSBONES SITE

Why is this site important?

- It is a large vacant site close to Borough High Street which provides a gap in the Southwark Street frontage.
- This site contains important archaeological remains.
- The site is located in an area of open space deficiency and provides an opportunity to create a new public open space in the area.

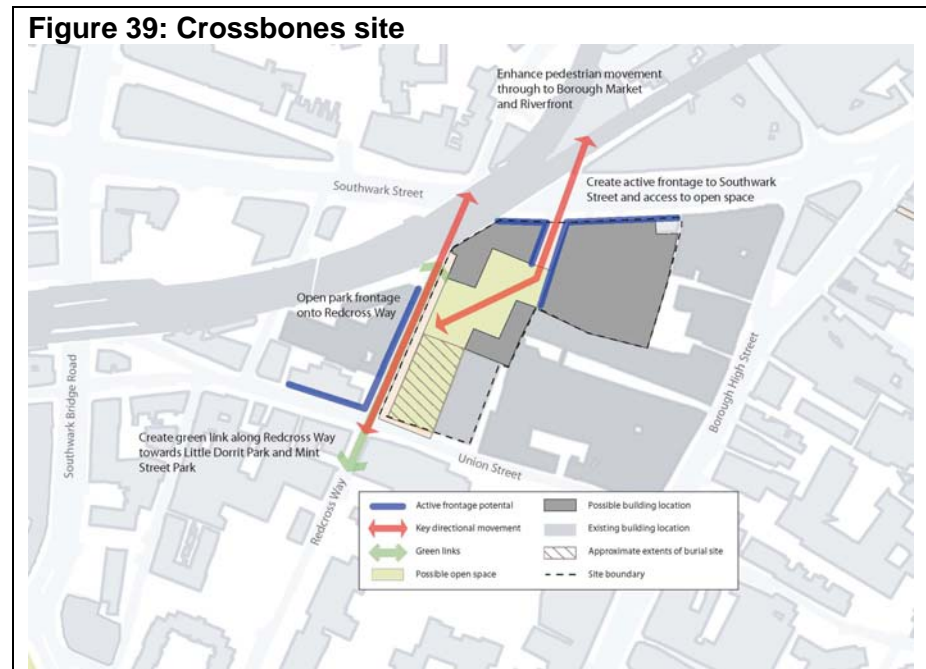
What issues need to be addressed?

- Consideration to archaeological remains must be given to ensure that open space adequately provides additional open space for the area whilst retaining sense of history and local importance. This could be developed through a memorial or historical interpretation.

- The site has frontage to busy Southwark Street and quiet Redcross Way.
- Improving pedestrian environment along Redcross Way and linking to existing improvement scheme.
- Short-term use of the site for construction of Thameslink Project.

How should the site be developed?

Figure 39 illustrates the redevelopment principles for the site.



Land Use

- Part of the site could be redevelopment for a mix use scheme providing residential uses, retail (A Class), B Class uses, community, leisure or cultural (D Class) uses.

- Part of the site should be developed as open space.

Movement

- Development should provide public access through the site from Southwark Street improving north-south links from Borough Market and Borough High Street to Redcross Way and Union Street.
- Development should improve links between nearby green spaces Mint St Park and Little Dorritt Park.

Built Form

- Development should be of high quality architecture and relate well to the public spaces provided.
- Building elements should be located in the north of the site along Southwark Street and should follow the established building line providing definition and enclosure to the street.
- Building heights should relate to established heights of the location.
- A full archaeological evaluation of the site will be required and this should inform the design of the scheme.

Public Realm

- Open space should be provided to the south of the site over the location of the burial remains. This space should have open frontage into Redcross Way and Union Street.
- An open link should be provided between the open space and Southwark Street.
- Active frontages should be provided to Southwark Street and along the public realm.
- The open space should be designed to interpret the archaeological importance of the site.

6. Implementation and Infrastructure

The previous sections of this document have set out the type of place we want Bankside, Borough and London Bridge to be. While there are many opportunities to regenerate and improve the area, we need to make sure that the approach we want to take is realistic and is able to be achieved with the resources and time that we have.

Developments are monitored every year in the annual monitoring report so that we can review the impact of this SPD/OAPF and decisions made on planning applications. We use this information to change our policies and decision making where needed.

This section explains how we will make sure that we can achieve the improvements we want to see in the area and how we will deliver the infrastructure and facilities needed to support growth.

6.1 Partnership working

We cannot deliver change alone. Much of the development and improvements expected in the area will be funded and delivered by private developers and landowners and a number of other organisations.

We will continue to work closely with the following organisations which are critical to the delivery of the vision for Bankside, Borough and London Bridge including:

- Greater London Authority
- London Development Agency
- Design for London
- Transport for London
- English Heritage

- Guy's Hospital
- Kings College
- Network Rail
- Registered Social Landlords
- Better Bankside
- Team London Bridge
- Cross River Partnership
- Bankside Open Spaces Trust
- Bankside Residents Forum

We have established good partnerships with these organisations and will maintain these to make sure that Southwark continues to be a good place to invest.

Fact Box: Business Improvement Districts

Business Improvement Districts (BIDs) are funded by businesses and work with council and other organizations to bring improvements to the local environment and economic performance of an area.

There are two BIDs in Bankside, Borough and London Bridge:

- Better Bankside, covering Borough High Street and the commercial area north of Southwark Street.
- Team London Bridge, covering the commercial area around London Bridge Station and Tooley Street

6.1.1 Working together to manage the impact of construction

With the amount of development likely to occur in the area in the coming years we need to ensure that construction activity is undertaken in a manner that protects as far as possible the safety, environment, health, well being and livelihoods of residents and businesses.

In order to coordinate development activity the London Bridge and Bankside Development Team has been set up. It will work closely with Better Bankside and Team London Bridge to help the smooth delivery of the development process and protect the amenity of the local community.

The development team approach will ensure that all relevant departments are brought into the process at an early stage and that dialogue is established with the developer, builder or architect and community in order to avoid unnecessary delays in the process.

The development team will aim to provide a one stop shop service, with officers being available to meet and discuss the proposals and becoming a part of the design team along with the developer, builder, and architect. The aim will be to create an agreed and speedy means of liaising between the various people involved in the design and construction of the project.

The team publishes and updates the following information:

- Map and timeline of all construction, public realm and utility projects.
- Monthly "d-brief" bulletin and quarterly newsletter highlighting all construction activity.
- Regular project updates to the community using as many forms of communication as possible, including email bulletins, newsletters, presentations, and events

This information is available at www.southwark.gov.uk/londonbridgedevlopment.

6.2 Community Involvement

It is essential that we continue to engage with communities to make sure we deliver their aspirations and to make the most of our community resources. We have prepared this document through a continuing process of dialogue of consultation with the community (see the consultation report for more information).

Southwark's Statement of Community Involvement (SCI) sets out how and when the community will be involved in preparing and reviewing planning documents and making decisions on planning applications.

It is important that the community is involved in the planning and design of individual schemes. We expect developers to engage the community at an early stage before an application is submitted and provide details of how they have done this in their Design and Access Statement.

6.3 Infrastructure

This SPD/OAPF identifies the potential for a significant level of new development over the next ten to fifteen years that will change the look and feel of the area and bring in more residents, workers and visitors. To make sure this creates successful places we need to make sure that there is investment in the public realm, social and economic infrastructure, utilities, and transport so that it can cope with growth.

In section 4 we identified important projects that will help accommodate the level of growth and change that new development will bring and make sure that it has positive

regeneration benefits that are spread across the whole area.

These projects have been identified through research and studies and in consultation with the local community. They include strategic projects that the Mayor of London has identified as important to the continued prosperity of London as well as more local projects important to improving the places in Bankside, Borough and London Bridge and managing the impact of increased development on local communities.

6.3.1 Community Project Banks

The council has recently piloted a Project Bank scheme through Community Councils. This has identified the community's priorities for improvements to local areas which do not necessarily need to be funded by new development. The list of projects and priorities will be updated regularly and will help inform the way development schemes should contribute to infrastructure and improvements in local areas.

More information at www.southwark.gov.uk/YourServices/planningandbuildingcontrol/S106/communityprojectbanks.html

6.3.2 Paying for the infrastructure

Delivering much of the infrastructure and improvements to the area will rely heavily on investment from developers as part of major development schemes and through planning contributions. Funding will also come from range of sources including:

- Business improvement districts
- Cleaner, Greener, Safer Programme
- Grant schemes such as Design for London's environment Fund and the Lottery Fund
- Transport for London grants

6.4 Securing the requirements

The funding and delivery of much of the necessary infrastructure, facilities and regeneration benefits in the area will be secured via Sections 106 planning obligation agreements attached to planning permissions.

Planning Obligations are agreements entered into by a developer to help address the impacts of a development. These are used to remove or reduce negative impacts of development proposals that are otherwise acceptable. They often involve making monetary payments to the council to pay for facilities which help overcome the negative impacts or carrying out works.

Planning Obligations have already helped to secure a range of improvements to community facilities, streets and open spaces and traffic and transport infrastructure in the area as well as helping pay employment and training schemes to get local people into jobs.

Southwark's S106 Planning Obligations Supplementary Planning Document provides further information on the approach we will take to securing planning obligations and contributions and sets out standard charges that will form the basis of negotiations with developers.

6.4.1 Planning obligation priorities

Priorities for how S106 Planning Obligations will be negotiated and collected have been set out by both the Mayor of London and community councils within Southwark.

The Mayor has identified affordable housing and transport as being generally of the highest priority, with learning and skills, health facilities and childcare provisions also being given priority.

Locally, the following priorities have been identified by Borough and

Bankside Community Council:

1. Increasing the quality and quantity of open spaces
2. Increasing the quality of public realm
3. Improving community safety and reducing crime
4. Improving community and leisure facilities
5. Increasing access to employment through training and other schemes

Bermondsey Community Council has identified the following priorities:

1. Increasing the quality and quantity of open spaces
2. Increasing the quality and quantity of community facilities
3. High quality of learning (schools and other channels)
4. Traffic/transport improvements
5. Improve town centre environment and retail offer

6.4.2 S106 framework for the opportunity area

The table below sets out our general approach to securing planning obligations in the opportunity area. It is a guide only and will be subject to negotiations as part of each individual planning application.

It is based on the standard planning obligation tariffs set out in

Southwark's S106 Planning Obligations Supplementary Planning Document, but with variations based on the particular issues and priorities in the area.

Other planning obligations may also be appropriate to mitigate the site-specific impacts of a particular development proposal.

Where appropriate or necessary, contributions will be 'pooled' and held by the council in a 'ring-fenced' fund until sufficient funds become available to deliver a project. The Council will reserve the right to redirect funds held in this manner to the implementation of another of the key projects if no further contributions towards a specific project come forward within a period of 3 years.

The government has consulted recently on introducing a community infrastructure levy. Our approach to planning obligations may change based on new requirements that may be introduced. Over the lifetime of this SPD/OAPF, we will manage our requirements for planning obligations to make sure that they remain up-to-date and that they meet any changing requirements of the government or the Mayor.

Theme	Approach	Justification and priorities
Affordable Housing	As per S106 SPD. The preference is for affordable housing to be delivered on-site as part of mixed and balanced schemes.	In exceptional circumstances where in-lieu payments are justified, contributions will be used for projects that will increase the supply of good quality affordable housing in Bankside, Borough and London Bridge, including the hidden homes programme.
Strategic Transport	At least the amounts set out in the S106 SPD. Contributions will be collected from all major development schemes and will be pooled.	Intensity of development will place pressure on transport infrastructure with a need to invest in it to improve capacity.

Theme	Approach	Justification and priorities
	<p>Contributions to strategic transport will be pooled for expenditure on the programme of projects set out in the LIP and Mayor's Transport Strategy.</p> <p>This will be reviewed following the outcomes of transport modeling for the area being undertaken by Transport for London and a decision on a possible tariff on new commercial floor space to help fund Crossrail.</p>	<p>Strategic projects are set out in section 4.4.2. Contributions will also be used to help fund the public realm projects with an important transport benefit a set out in section 4.3. Key projects are:</p> <ul style="list-style-type: none"> - Upgrade to station and bus interchange at London Bridge - Crossrail - Blackfriars Road Improvement scheme - Borough High Street Improvement scheme - Stoney Street/Winchester Walk transport and streetscape improvement scheme - Cycling network improvements - Improving network of pedestrian links - Junction and crossing improvements
Open Space	<p>At least the amounts set out in the S106 SPD, including additional contribution for development in areas of district park deficiency.</p> <p>Contributions will be collected from all major development schemes including housing, student homes, employment generating uses and visitor generating development.</p> <p>Contribution will be pooled and used to fund the projects set out in section 4.3. This will be reviewed following completion of the Southwark Open Spaces Strategy.</p>	<p>There is a network of small and scattered open spaces in the area. There is limited access to large green spaces. Intensity of development will place pressure on the existing network of green spaces. Key projects are:</p> <ul style="list-style-type: none"> - New Crossbones open space - Little Dorrit Park improvements - St James churchyard refurbishment - St Mary Magdalene Churchyard refurbishment - Nelson Square improvements - Webb Street Park improvements - Tanner Street park improvements - St John's churchyard extension - Mint Street Adventure Playground and Park improvements - Community swimming pool at Elephant and Castle
Play and sports development	At least the amounts set out in the S106 SPD.	Intensity of development will place pressure on existing facilities:

Theme	Approach	Justification and priorities
	<p>Plus contributions will be collected to fund the operation and maintenance of facilities.</p> <p>Contributions will be collected from all major development schemes including housing and employment generating.</p> <p>Contribution will be pooled and used to fund the projects set out in section 4.3.</p>	<ul style="list-style-type: none"> - Community swimming pool at Elephant and Castle - Improvements to and provision of new play facilities and play grounds across the area.
Public Realm	<p>As per S106 SPD, the contribution will be negotiated based on the specific impacts of each scheme and opportunity for development to implement elements of the public realm strategy set out in section 4.3.</p>	<p>Intensity of development will place pressure on the existing public realm. The public realm strategy in section 4.3.</p> <p>Key projects are:</p> <ul style="list-style-type: none"> - Bankside Urban Forest Schemes - Borough and Bankside Streetscape Improvement schemes - Bermondsey Streetscape Improvement schemes - Improving pedestrian permeability - Light at the End of the Tunnel Schemes - Green links between open spaces - Improvements to Borough High Street - Improvements to St Thomas Street - Improvements to Snowsfields - Public realm improvements around London Bridge Station - Improving permeability of Guys Hospital and King's College - Legible London project
Community Facilities	<p>At least the amounts set out in the S106 SPD. Plus contributions will be collected to fund the operation and maintenance of facilities.</p> <p>Contributions will be collected from major residential</p>	<p>Intensity of development will place pressure on community facilities and there will be a need to provide a range of services to the local community. Priorities for funding are set out in section 4.1.8 and also have been identified through the Community</p>

Theme	Approach	Justification and priorities
	schemes. Contributions will be pooled towards projects that will benefit the whole area.	Project Bank. Key projects are: <ul style="list-style-type: none"> - Continued support and expansion of youth facilities and service providers, including the Rockingham Centre. - Implementing phases 2b and 2c of the Whites Ground skate park improvement scheme to create a new youth facility. - A new community centre at Mint Street.
Local Transport	As per S106 SPD, the contribution will be negotiated based on the specific impacts of each scheme and opportunity for development to implement elements of the public realm strategy set out in section 4.3 and projects identified in the LIP.	Intensity of development will place pressure on local streets and the highway network. Key projects are: <ul style="list-style-type: none"> - Road safety and speed projects - Car clubs - Controlled parking zones
Education	We will collect the contribution towards school provision as set out in the S106 SPD. This will be required from major residential development schemes containing family housing. Financial contributions will be pooled.	The Southwark Schools for the Future sets t plans for investment in children’s centres, primary schools, secondary schools and academies. In addition, our projections show that from 2018 there will also be the need for a new two form of entry primary school to support a growing population.
Employment and Enterprise	At least the amounts set out in the S106 SPD. Plus we will secure contributions towards affordable business space and business support schemes. This will be required from major commercial schemes, including hotels, entertainment, arts and cultural facilities. Major residential schemes will be required to contribute towards construction employment schemes. Financial contributions will be pooled.	There is a need to make sure that small and medium sized businesses are not excluded from the employment floor space being built in the area. Cathedrals and Riverside wards generate a very high proportion of new jobs in Southwark. It is important that local people are able to access the jobs created by new development in the area, so that it does not just benefit visitors and workers. Contributions will be used for a range of projects including: <ul style="list-style-type: none"> - Improving local retail environments

Theme	Approach	Justification and priorities
		<ul style="list-style-type: none"> – Employment and training schemes – Business support – Affordable and flexible business space – Skills and training – Building London Creating Futures – The SART – Southwark Works – Business Extra and Southwark Business Desk – support to businesses. <p>More detail on these schemes is provided in appendix 8.</p>
Archaeology	As per S106 SPD.	Much of Bankside, Borough and London Bridge is within an archaeological priority zone.
Health	As per S106 SPD.	Guys Hospital is planned to be improved to provide both local and strategic health services. We are investigating the possibility of using contributions for projects that help promote good health and reduce the burden on existing health infrastructure.
Energy	S106 obligations will be used to secure connection to local energy networks.	
Admin	As per S106 SPD.	

Appendix 1 – Planning Framework

A1.1 Where does the SPD document fit in?

The main document which is currently used to guide development in Southwark is the Southwark Plan which was adopted in 2007.

In 2004 the Government made changes to the planning system and required all councils to produce a new set of planning documents, called the local development framework (LDF). The local development framework contains a number of different planning documents and is illustrated in Figure A1.1 below. The local development framework will eventually replace the Southwark Plan.

The most important document in the folder is the Southwark Core Strategy. The core strategy sets out how Southwark will change up to 2026 to become the type of place set out in our Sustainable Community Strategy (Southwark 2016). It provides our long term vision, spatial strategy and strategic policies to make sure development has all the right elements to help create deliver successful and sustainable places. You can find out more information about the core strategy at: www.southwark.gov.uk/corestrategy

All of the planning documents in the local development framework including the Bankside, Borough and London Bridge SPD need to be consistent with the core strategy. As an SPD, this document does not create new policy, but provides detailed guidance on how our current planning policies will be applied in the opportunity area. These policies are set out in the London Plan (consolidated with alterations) 2008, the Southwark Plan 2007 and

Southwark Core Strategy (publication/submission version). The relevant policies are set out in Appendix 2.

The SPD will be an important tool in deciding planning applications in Bankside, Borough and London Bridge.

The Bankside, Borough and London Bridge SPD will only apply in full when the core strategy is adopted. Before this time, it will still be important (a material consideration) and will have some influence over decisions on planning applications.

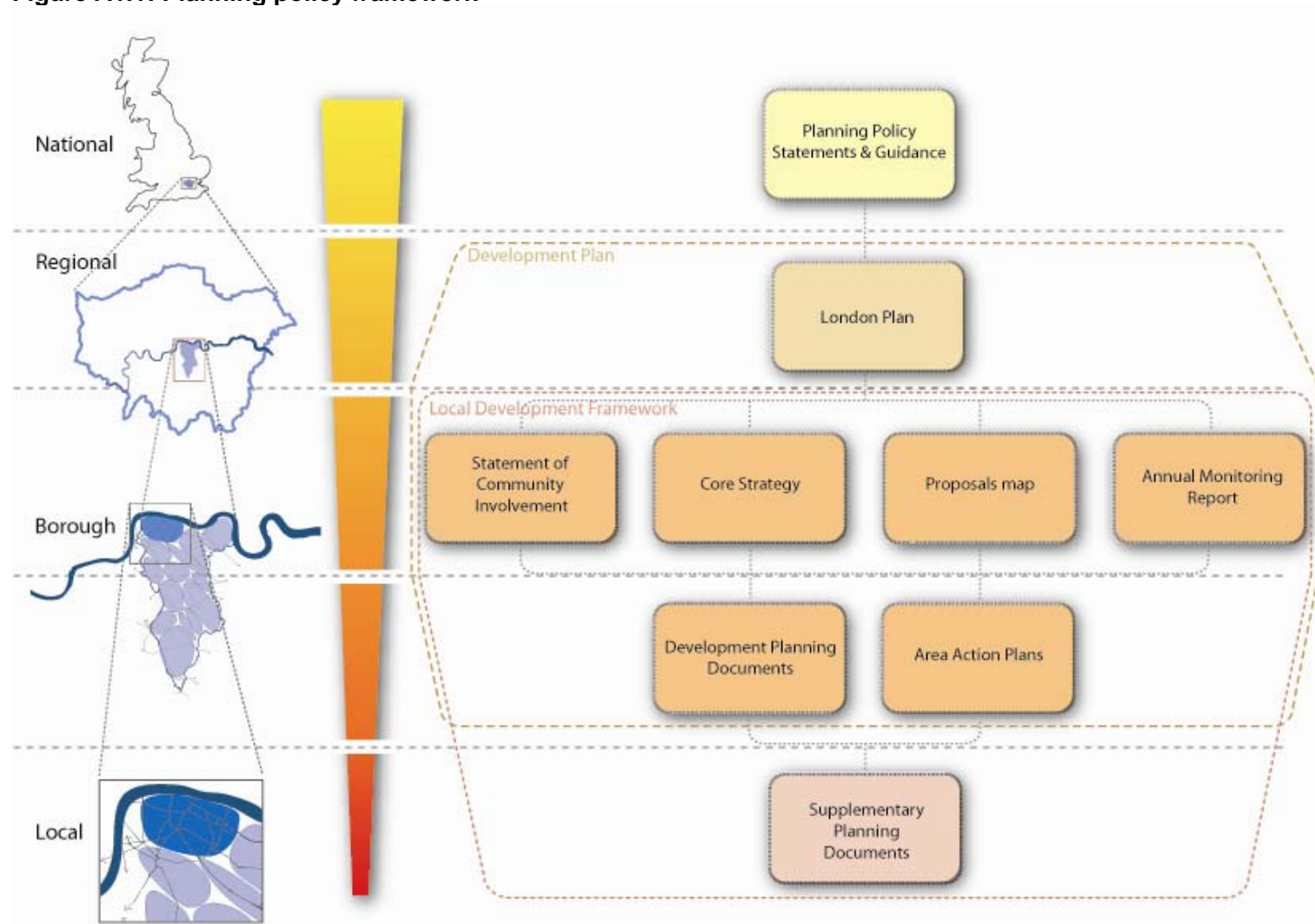
A1.2 The Mayor of London's planning policies

In addition to the core strategy, this document needs to be consistent with the Mayor's policies in the London Plan, as well as national planning policies. The London Plan sets out how a broad strategy for how London will change and develop over the next 20-25 years. This includes identifying parts of London that have considerable scope for employment and housing growth to help make the capital a successful city. These are known as opportunity areas and include Bankside, Borough and London Bridge, as well as Elephant and Castle.

The Mayor can adopted opportunity area planning frameworks to provide further detail on how development should occur in the opportunity areas. These will be used when deciding strategic planning applications referred to the Mayor.

The Mayor is currently consulting on a draft replacement to the London Plan. The emerging policies in the draft replacement London Plan 2009 have been taken into account in preparing this document.

Figure A1.1: Planning policy framework



A1.3 Documents that will be replaced by the SPD

This document replaces the following documents which will be rescinded once the SPD is adopted:

- Draft London Bridge Planning Framework October 2002
- Draft London Bridge Opportunity Area Supplementary Planning Guidance November 2002
- Draft Bankside and the Borough Supplementary Planning Guidance November 2002

A1.4 Other planning documents that are also relevant

This SPD should be used with other planning documents that provide guidance on other aspects of development, including:

- Sustainable Design and Construction Supplementary Planning Document: this document sets out the environmental standards and design principles for the new development in the area.
- Sustainability Assessment Supplementary Planning Document: this document explains the information that will be required to demonstrate the environmental impact of a development and how this will be balanced with other sustainability objectives.
- Design and Access Statements Supplementary Planning Document: this document explains how to prepare design and access statements, which need to be submitted with most planning applications.
- Residential Design Standards Supplementary Planning Document: this document explains minimum design standards for residential development to ensure it is of a high quality and meets housing need in the borough.
- S106 Planning Obligations Supplementary Planning Document: this document sets out the common planning

obligations the council will seek to help make developments more sustainable.

- Transport SPD: sets out requirements for transport plans and assessments.

Development in areas surrounding the opportunity area will be guided by other adopted planning documents including:

- Elephant and Castle Enterprise Quarter SPD 2008
- Elephant and Castle Development Framework 2004
- Waterloo Area SPD 2009

Please make sure that you check our website to ensure you use our most up-to-date planning policies and guidance:

www.southwark.gov.uk/YourServices/planningandbuildingcontrol/planningpolicy

Appendix 2: Planning policies relating to this document:

The table below sets out the planning policies which are relevant to each section of the SPD. These appendices will be updated as policies change.

SPD section	London Plan	Draft replacement London Plan	Core Strategy	Saved Southwark Plan
Vision	2A.4 The Central Activities Zone 2A.5 Opportunity Areas 4B.8 Respect local context and communities 5D.1 The strategic priorities for South East London 5D.2 Opportunity Areas in South East London 5G.2 Strategic priorities for the Central Activities Zone	Policy 2.10 Central Activities Zone – strategic priorities Policy 2.11 Central Activities Zone – strategic functions Policy 2.12 Central Activities Zone – predominantly local activities Policy 2.13 Opportunity Areas and Intensification Areas Policy 7.29 The River Thames	Strategic Targets Policy 2 - Improving places Central Activities Zone Bankside, Borough and London Bridge opportunity area Strategic Policy 1 – Sustainable development	7.4 Bankside and Borough Action Area 8.3 London Bridge
Land use	2A.1 Sustainability criteria 3A.3 Maximising the potential of sites 3D.1 Supporting town centres 3D.2 Town centre development 4B.8 Respect local context and communities	Policy 2.10 Central Activities Zone – strategic priorities Policy 2.11 Central Activities Zone – strategic functions Policy 2.12 Central Activities Zone – predominantly local activities Policy 2.15 Town Centres Policy 7.1 Building London's neighbourhoods and communities Policy 7.15 Reducing noise and enhancing soundscapes	Strategic Policy 1 – Sustainable development Strategic Policy 3 – Shopping, leisure and entertainment Strategic Policy 12 – Design and conservation	Policy 1.7 Development within town and local centres Policy 1.8 Location of developments for retail and other town centre uses Policy 3.1 Environmental Effects Policy 3.2 Protection of Amenity
Retail	2A.8 Town centres 3D.1 Supporting town centres 3D.3 Maintaining and improving retail facilities 5G.4 Central Activities: Retailing	Policy 2.15 Town Centres Policy 4.7 Retail and town centre development Policy 4.8 Supporting a successful and diverse retail sector	Strategic Policy 3 – Shopping, leisure and entertainment	Policy 1.7 Development within town and local centres Policy 1.8 Location of developments for retail and other town centre uses Policy 1.9 Change of use

SPD section	London Plan	Draft replacement London Plan	Core Strategy	Saved Southwark Plan
	5G.5 Predominantly Local Activities in the Central Activities Zone	Policy 4.9 Small shops		within protected shopping frontages Policy 1.10 Small scale shops and services outside the town and local centres and protected shopping frontages
Restaurants, cafes, drinking establishments and gaming premises	3D.1 Supporting town centres 3D.3 Maintaining and improving retail facilities	Policy 2.15 Town Centres Policy 4.7 Retail and town centre development Policy 4.8 Supporting a successful and diverse retail sector	Strategic Policy 3 – Shopping, leisure and entertainment	Policy 1.7 Development within town and local centres Policy 1.8 Location of developments for retail and other town centre uses
Business space	3B.2 Office demand and supply 3B.8 Creative industries 3B.11 Improving employment opportunities for Londoners 5D.2 Opportunity Areas in South East London 5G.3 Central Activities: Offices	Policy 2.15 Town Centres Policy 4.1 Developing London's Economy Policy 4.2 Offices Policy 4.3 Mixed use development and offices Policy 4.12 Improving opportunities for all	Strategic Policy 10 – Jobs and businesses	Policy 1.1 Access to employment opportunities Policy 1.4 Employment sites
Arts, cultural and entertainment uses	3D.1 Supporting town centres 3D.4 Development and promotion of arts and culture 4C.17 Thames Policy Area	Policy 2.15 Town Centres Policy 4.6 Support and enhance the provision for arts, culture and entertainment	Strategic Policy 3 – Shopping, leisure and entertainment Strategic Policy 4 – Places to learn and enjoy Strategic Policy 10 – Jobs and businesses	Policy 1.11 Arts, culture and tourism uses Policy 1.12 Hotels and Visitor accommodation Policy 2.1 Enhancement of community facilities Policy 2.2 Provision of new community facilities Policy 3.29 Development within the Thames Policy Area Policy 3.30 Protection of riverside facilities
Hotels	3D.7 Visitor accommodation	Policy 4.5 London's visitor	Strategic Policy 10 – Jobs	

SPD section	London Plan	Draft replacement London Plan	Core Strategy	Saved Southwark Plan
	and facilities	Infrastructure	and businesses	
Purpose built student accommodation	3A.5 Housing choice	Policy 3.8 Housing choice	Strategic Policy 8 – Student homes	Policy 4.7 Non-self contained housing for identified user groups
Housing	3A.1 Increasing London's supply of housing 3A.2 Borough housing targets 3A.3 Maximising the potential of sites 3A.5 Housing choice 3A.6 Quality of new housing provision 3A.7 Large residential developments 5D.2 Opportunity Areas in South East London	Policy 3.3 Increasing housing supply Policy 3.4 Optimising housing potential Policy 3.5 Quality and design of housing developments Policy 3.7 Large residential developments Policy 3.8 Housing choice Policy 3.13 Negotiating affordable housing on individual private residential and mixed use schemes	Strategic Policy 5 – Providing new homes Strategic Policy 6 – Homes for people on different incomes Strategic Policy 7 – Family homes	Policy 3.11 Efficient use of land Policy 4.1 Density of Residential Development Policy 4.2 Quality of residential accommodation Policy 4.3 Mix of dwellings Policy 4.4 Affordable housing Policy 4.5 Wheelchair affordable housing Policy 4.6 Loss of residential accommodation Policy 4.7 Non-self contained housing for identified user groups
Community facilities	3A.18 Protection and enhancement of social infrastructure and community facilities 3D.6 The Olympic and Paralympic Games and sports facilities	Policy 3.6 Children and young people's play and informal recreation facilities Policy 3.17 Protection and enhancement of social infrastructure Policy 3.20 Sports facilities Policy 7.27 Blue Ribbon Network: supporting infrastructure and recreational use	Strategic Policy 4 – Places to learn and enjoy	Policy 2.1 Enhancement of community facilities Policy 2.2 Provision of new community facilities
Built form and urban design	3A.3 Maximising the potential of sites 4B.1 Design principles for a	Policy 2.18 Green infrastructure: the network of open and natural spaces	Strategic Policy 2 – Sustainable transport Strategic Policy 12 – Design	Policy 3.11 Efficient use of land Policy 3.12 Quality in design

SPD section	London Plan	Draft replacement London Plan	Core Strategy	Saved Southwark Plan
	compact city 4B.2 Promoting world-class architecture and design 4B.3 Enhancing the quality of the public realm 4B.8 Respect local context and communities 4B.9 Tall buildings – location 4B.10 Large-scale buildings – design and impact 4B.11 London’s built heritage 4B.12 heritage conservation 4B.14 World Heritage Sites 4B.15 Archaeology 4C.17 Thames Policy Area 4B.16 London View Management Framework 4B.18 Assessing development impact on designated views 4C.11 Increasing access alongside and to the Blue Ribbon Network 3C.21 Improving conditions for walking	Policy 5.10 Urban greening Policy 6.10 Walking Policy 7.2 An inclusive environment Policy 7.3 Secured by design Policy 7.4 Local character Policy 7.5 Public realm Policy 7.6 Architecture Policy 7.7 Location and design of tall and large buildings Policy 7.8 Heritage assets and Archaeology Policy 7.10 World Heritage Sites Policy 7.11 London View Management Framework Policy 7.12 Implementing the London View Management Framework Policy 7.29 The River Thames	and conservation	Policy 3.13 Urban design Policy 3.14 Designing out crime Policy 3.15 Conservation of the historic environment Policy 3.16 Conservation areas Policy 3.17 Listed buildings Policy 3.18 Setting of listed buildings, conservation areas and world heritage sites Policy 3.19 Archaeology Policy 3.22 Important Local Views Policy 3.23 Outdoor Advertisements and signage Policy 3.26 Borough open land Policy 3.27 Other open space Policy 5.3 Walking and cycling
Detailed requirements for tall building applications	3C.2 Matching development to transport capacity 4B.2 Promoting world-class architecture and design 4B.9 Tall buildings – location 4B.10 Large-scale buildings – design and impact 4B.14 World Heritage Sites 4B.16 London View Management Framework	Policy 7.7 Location and design of tall and large buildings Policy 7.10 World Heritage Sites Policy 7.11 London View Management Framework Policy 7.12 Implementing the London View Management Framework	Strategic Policy 12 – Design and conservation Strategic Policy 11 – Open spaces and wildlife	Policy 3.16 Conservation areas Policy 3.18 Setting of listed buildings, conservation areas and world heritage sites Policy 3.20 Tall buildings Policy 3.22 Important Local Views Policy 3.31 Strategic views

SPD section	London Plan	Draft replacement London Plan	Core Strategy	Saved Southwark Plan
	4B.18 Assessing development impact on designated views			
Traffic and Transport	3C.1 Integrating transport and development 3C.2 Matching development to transport capacity 3C.4 Land for transport Policy 3C.20 Improving conditions for buses 3C.22 Improving conditions for cycling 3C.23 Parking strategy 3C.24 Parking in town centres 4C.7 Passenger and tourism uses on the Blue Ribbon Network	Policy 6.1 Strategic approach Policy 6.2 Providing public transport capacity and safeguarding land for transport Policy 6.3 Assessing transport capacity Policy 6.4 Enhancing London's transport connectivity Policy 6.7 Buses, bus transits, trams Policy 6.8 Coaches Policy 6.9 Cycling Policy 6.11 Smoothing traffic flow and tackling congestion Policy 6.12 Road network capacity Policy 6.13 Parking Policy 7.25 Increasing the use of the Blue Ribbon Network for passengers and tourism Policy 7.26 Increasing the use of the Blue Ribbon Network for freight transport Policy 7.27 Blue Ribbon Network: supporting infrastructure and recreational use	Strategic Policy 2 – Sustainable transport	Policy 5.2 Transport impacts Policy 5.3 Walking and cycling Policy 5.4 Public transport improvements Policy 5.6 Car parking Policy 5.7 Parking standards for disabled people and the mobility impaired Policy 5.8 Other parking
Environmental considerations	3D.14 Biodiversity and nature conservation	Policy 5.1 Climate change mitigation	Strategic Policy 11 – Open spaces and wildlife	Policy 3.3 Sustainability Assessment

SPD section	London Plan	Draft replacement London Plan	Core Strategy	Saved Southwark Plan
	4A.1 Tackling climate change 4A.3 Sustainable design and construction 4A.4 Energy assessment 4A.5 Provision of heating and cooling networks 4A.6 Decentralised energy: Heating, Cooling and Power 4A.7 Renewable energy 4A.16 Water supplies and resources	Policy 5.2 Minimising carbon dioxide emissions Policy 5.3 Sustainable design and construction Policy 5.5 Decentralised energy Networks Policy 5.6 Decentralised energy in development proposals Policy 5.7 Renewable energy Policy 5.9 Overheating and cooling Policy 5.11 Green roofs and development site environs Policy 5.12 Flood risk management Policy 5.13 Sustainable drainage Policy 5.15 Water use and supplies Policy 7.14 Improving air quality Policy 7.18 Protecting local natural space and addressing local deficiency Policy 7.19 Biodiversity and access to nature	Strategic Policy 13 – High environmental standards	Policy 3.4 Energy efficiency Policy 3.9 Water Policy 3.28 Biodiversity
Implementation and Infrastructure	6A.2 Working in partnership 6A.4 Priorities in Planning obligations 6A.5 Planning obligations	Policy 6.5 Funding Crossrail and other strategically important transport infrastructure Policy 8.1 Implementation Policy 8.2 Planning obligations	Strategic Policy 14 – implementation and delivery	Policy 2.5 Planning Obligations

Appendix 3: Social, environmental and economic profile of Bankside, Borough and London Bridge

A3.1 People

The opportunity area overlaps four wards, Cathedrals, Chaucer, Grange and Riverside. In 2007, the population of four wards was estimated at 15,508¹. The population is expected to grow by more than a third between now and 2026².

Around 24% of the population is less than 19 years old, which is similar to the Southwark of 25%.

At the 2001 census 18% of residents over 16 years old in the four wards were full time students (26% of the population in Cathedrals). This compares with 13% across Southwark.

While, overall, it is one of the least deprived areas in Southwark and London, there are pockets which still experience high levels of deprivation, particularly in crime, health, access to employment and housing.

The area has high levels of unemployment (thirteen percent) which suggests that local residents are not accessing the new jobs, though overall skill levels of residents are slightly higher than the rest of Southwark. 13% of the population has qualifications at grade levels 3 and 37% at levels 4/5. This compares to the Southwark average of 10% and 35% respectively.

Of those people employed who live in the area, a higher proportion of people work in managerial and professional

occupations in comparison with Southwark and the rest of the UK (Census, 2001).

The number of people claiming benefits in the opportunity area is lower than the average for Southwark. Of those claiming benefits, the highest proportion of claims are for incapacity job benefits, compared to job seekers allowance and lone parents benefits.

A3.2 Homes

There are around 11,000 homes in the opportunity area, most of which are 1, 2 and 3 bedroom flats. Currently, 70%-79% of housing is affordable (ie. owned by the council or and RSL), with the exception of the Riverside ward where 30-39% of units are affordable housing. The majority of the more recent development throughout the BBLB area has been for private occupation.

Whilst there is a high amount of affordable housing located in the area, there continues to be an identified need for more affordable housing in Southwark.

A3.3 Purpose built student accommodation

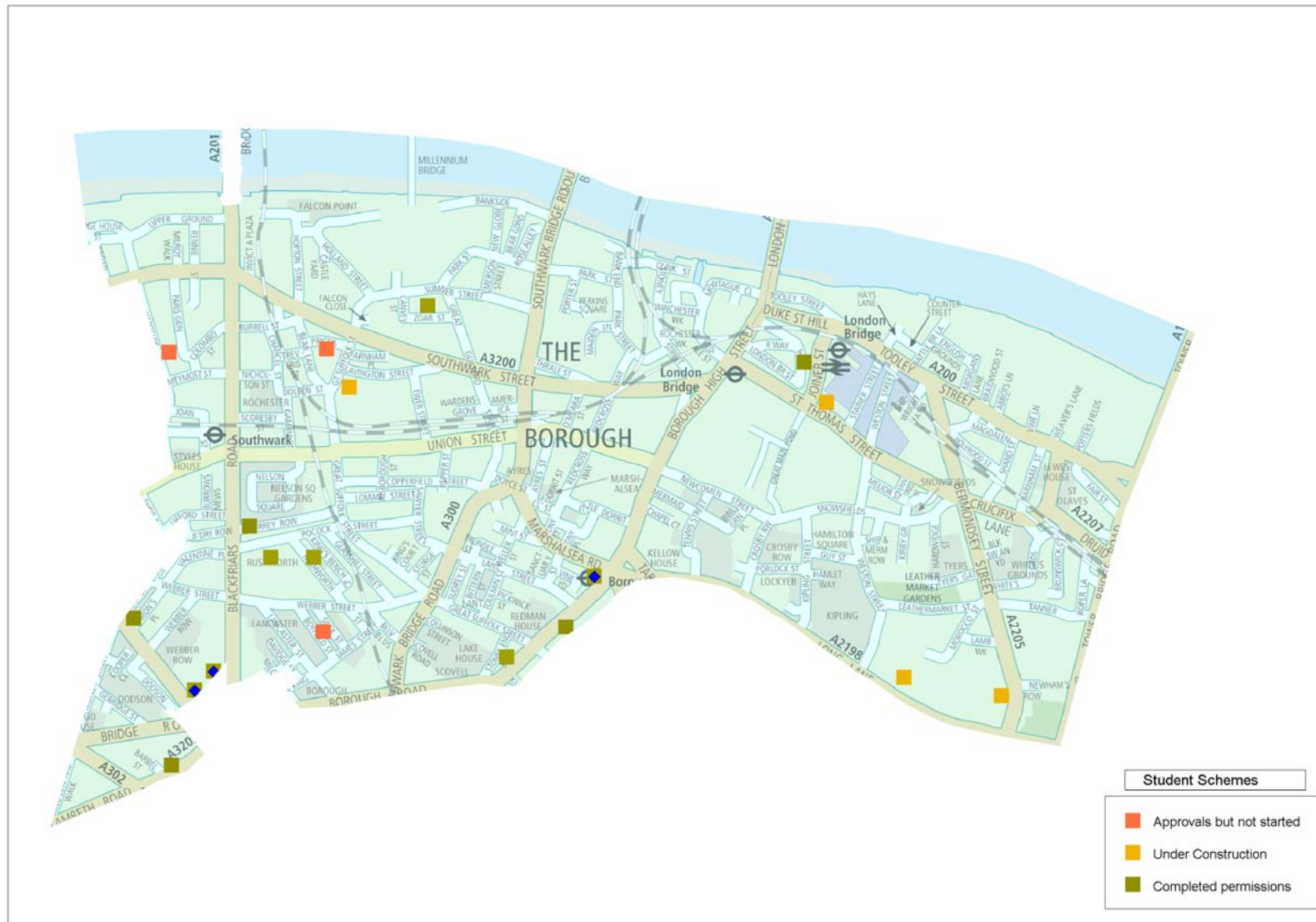
Southwark has the second largest number of student homes in London. Since 1991, more than 3,923 bedrooms in purpose built student accommodation have been built in Southwark, 2,991 bedrooms in the SE1 area (see Figure A3.1).

There is continued interest for large scale student homes in the area.

¹ 2007 GLA population estimate

² 2008 GLA population projections, low scenario

Figure A3.1 Student housing development in Bankside, Borough and London Bridge



A3.4 History

Borough, Bankside and London Bridge are among the most historic areas in London, with many layers of history. Unlike much of Southwark, the area has been built-up since Roman times, though it was settled as far back as 3000 BC. The Romans built the first London Bridge which allowed London to expand south across the river. Remains of Roman buildings, roads and industrial structures and areas have been found.

Over the years, there have been many types of land uses and the face of the area has been constantly changing as a result. It is the medieval period from which much of the area's historic townscape originates.

For many centuries outside the jurisdiction of the City, it became known for its inns and bawdy behaviour and as a haven for law breakers. Bankside was once an area of playhouses including the Rose, Swan and Globe. Two monuments from this period are scheduled, the remains of the Rose Theatre (the first theatre on Bankside) and the Globe (specifically associated with Shakespeare, both on Park Street.

With the growth of shipping and maritime London, the area became used for wharves and warehousing. Other industries to take off in the area included leather making and brewing.

The development of the railway in the nineteenth century also had a significant impact on the area, cementing its role as a major transport interchange linking to many parts of London and the country.

Since the 1980s there has been a very active development market with the warehouses and wharves being changed into homes, shops, offices and tourism uses.

The opportunity area's rich history is reflected in its listed buildings and conservation areas. There are over 100 Grade II listed buildings, approximately 5 Grade II* listed buildings, and 3 Grade I listed buildings. The Tower of London World Heritage site is also located nearby. There are eight conservation areas and four more close by (see figure A3.2). These are:

- Old Barge House Alley
- Bankside and Bear Gardens
- Union Street
- Thrale Street
- Borough High Street
- Tooley Street – north and south
- Bermondsey Street
- Kings Bench (proposed)

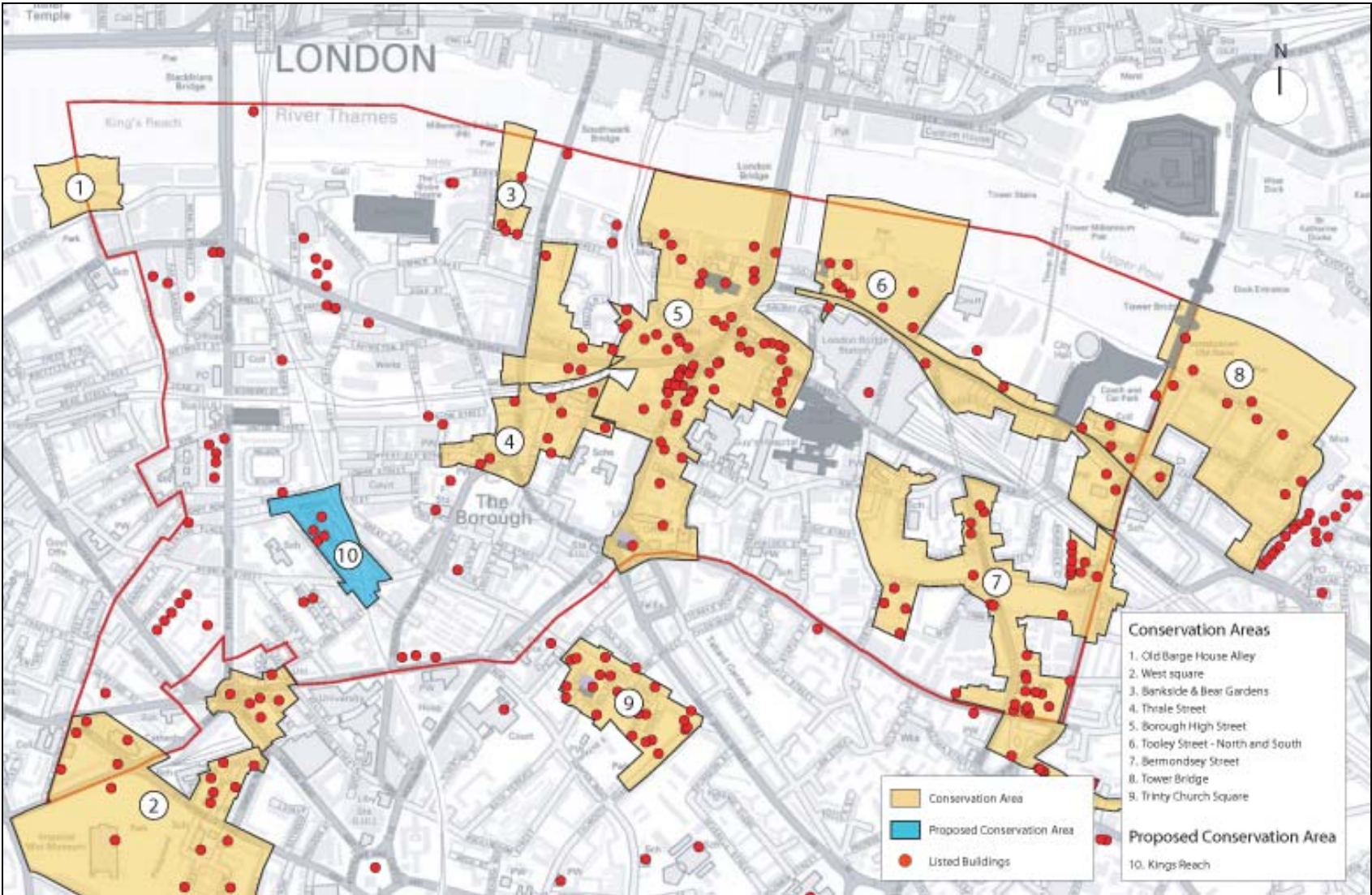
Bankside, Borough and London Bridge contains regionally and nationally important archaeological monuments and sites. As a result, much of the area is designated as an archaeology priority zone designation under the Southwark Plan and Core Strategy.

A3.5 Transport

Borough, Bankside and London Bridge has excellent transport connections to other parts of central London and the wider region by both road and rail. This includes:

- The Thameslink railway linking Brighton to Bedford
- The Jubilee and Northern Lines
- The River Thames with associated river services
- Numerous bus routes running through the area linking to other parts of London.
- Strategic cycle links
- Riverside walk linking Shad Thames to Vauxhall via the Thames Path.
- Millennium Bridge.

Figure A3.2 Conservation areas and listed buildings in Bankside, Borough and London Bridge



There are major transport interchanges at London Bridge and Blackfriars and just outside the area at Waterloo and Cannon Street.

London Bridge station is one of the most important transport hubs in London and one of the busiest stations in the country. It has seen the highest growth in passenger numbers of all the stations in London, with 115 million people passing through in 2008. This compares with approximately 80 million people who passed through Heathrow airport in 2008³. Passenger numbers are forecast to continue growing over the coming years as the area is regenerated. The Thameslink programme will increase the station's capacity by 2015.

The extension of the Jubilee Line into Southwark in 1998 has also brought major benefits to the area, opening up access to Canary Wharf and the West End. The area around Blackfriars Road and Bankside has particularly benefited from the creation of Southwark Station.

As part of the Thameslink programme, Blackfriars Station is being rebuilt and expanded across the river, providing a new station access from Bankside which will further improve access to transport.

The network of roads in the area is shown on Figure A3.4 **Error! Reference source not found.** Traffic through the area is mostly on a few major routes including:

- The A3 (Borough High Street) linking Monument to Elephant and Castle
- The A201 (Blackfriars Road) linking Farringdon to New Kent Road
- The A100 (Tower Bridge Road) linking Tower Hill to Bricklayers Arms

³ Heathrow, Surface Access Strategy, BAA, November 2007

- Stamford Street, Southwark Street, Tooley Street and St Thomas Street provide the main east-west movements through the area.

These major roads are the responsibility of Transport for London. They carry high volumes of traffic through the area, making use important river crossing at Tower Bridge, London Bridge, Southwark Bridge and Blackfriars Bridge. From the east London Bridge provides the first crossing points over the Thames.

The river itself is also an important link for freight and passengers. There are three ferry piers in the opportunity area and a fourth just to the west at Southbank (see Figure A3.4).

London Bridge station is a major trip generator for taxis, however, drop-off and pick-up facilities are poor and limited Coaches provide an important function for commuters and for tourism but existing stopping facilities are limited and there is currently a coach ban in the area behind the Tate Modern (see Figure A3.5).

Bus services are also important in the area, with many linking with major transport hubs at London Bridge, Waterloo and Blackfriars. Transport for London predicts an average increase in the use of bus services of 3% per year in central London, but this has been exceeded at London Bridge, where the increase has been 5% per year, and is likely to continue for the foreseeable future. There is a need to redesign and upgrade the bus station at London Bridge Station to help it better connect with rail services and cope with an increased flow of buses and taxis.

There is a cycle network through the area which provides dedicated paths (see Figure A3.5). This includes parts of National Cycle Highway Route 4 and planned Cycle Superhighways (see section 4.3).

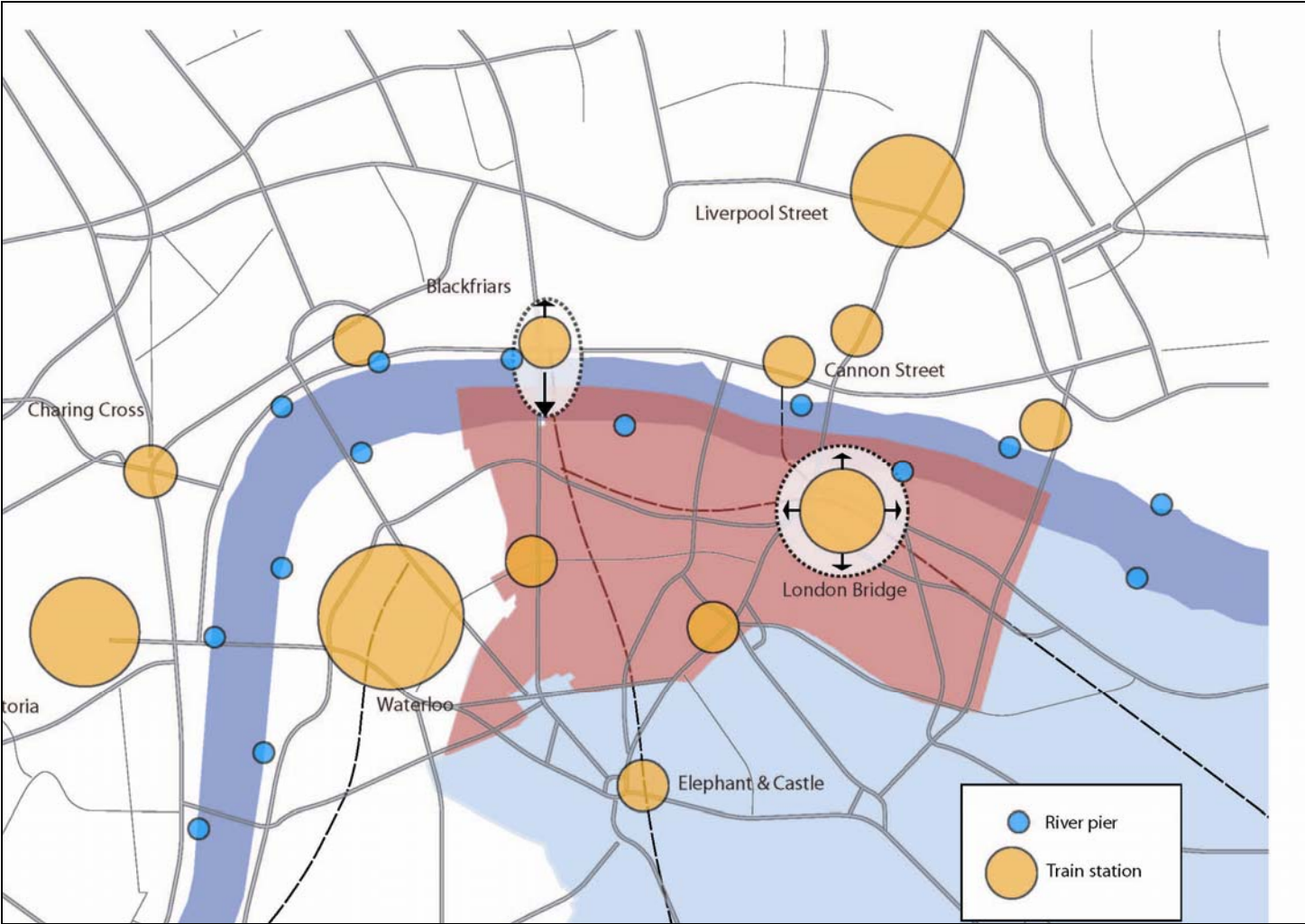


Figure A3.4: Transport network

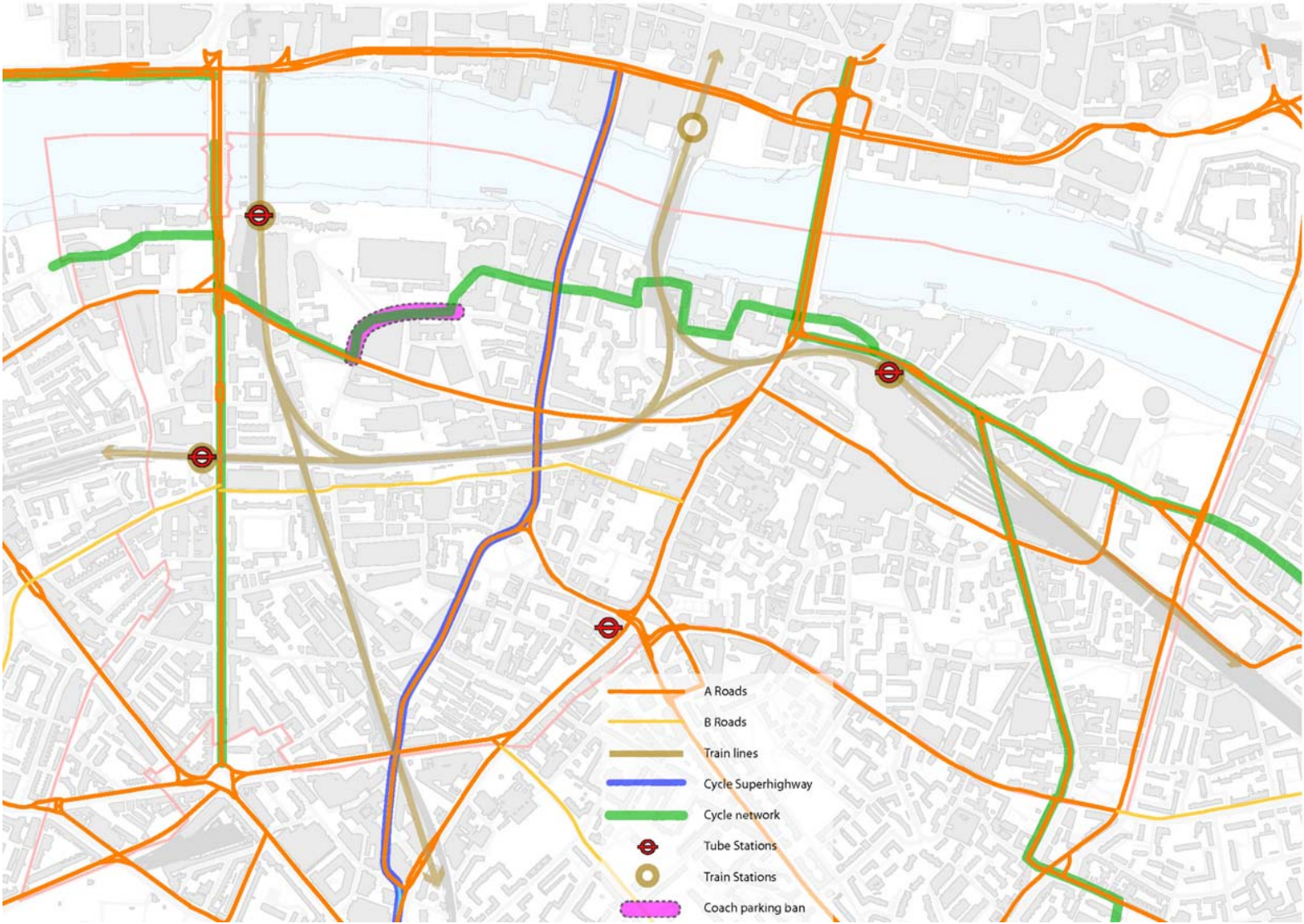


Figure A3.5: Road, rail and cycle network

Most of the pedestrian activity in the area takes place in the north between the railway viaducts and the River, where most of the business space and cultural attractions are located. The railway viaducts themselves act as a barrier and the tunnels through them can be dark and uninviting, although there have been improvements over recent years.

The effect of this is that the streets in the northern part of the area are often crowded and difficult to cross, making walking unpleasant.

A3.6 Enterprise

Bankside, Borough and London Bridge play an important role in enabling Southwark and London to grow and prosper. The opportunity area is part of the Central Activities Zone (CAZ) which contains nearly 30% of all London's jobs and is projected to accommodate 36% of London's employment growth up until 2026.

The Southwark Employment Land Review found that the majority of locally based businesses are searching for facilities of between 200sqm and 500sqm.

Bankside, Borough and London Bridge are the main employment and business areas in Southwark, containing 32% of business employing over 94,000 people. The opportunity area generates 63% of Southwark's wealth⁴. Between 2003

and 2007 57% of new jobs created in Southwark were in Cathedral and Riverside wards⁵.

Some of Southwark's largest employers are located in the area including Guy's Hospital, Greater London Authority, London Development Agency, Price Waterhouse Coopers, Financial Times, IPC. However most businesses are small and medium sized. Around 97% of the businesses in the AAP area are small businesses and 70% employ less than 10 people.

The area also contains major institutions including Guy's Hospital and King's College which are visited by over 15,000 patients and students each day (and forecast to increase to 20,000 visitors per day by 2020). Private medical colleges have also established around the hospital at London Bridge.

Since the 1980s commercial development in the area has been mainly offices, with demand for warehousing and manufacturing declining. The types of businesses locating in the area include government offices, company headquarters, support services for the financial sector in the City, professional bodies, communications, publishing, and advertising. Many of these activities have are important as they are at the heart of London's world city role.

Employment in hotels, catering, tourism and creative and cultural industries has also grown significantly. The area also has a large concentration of creative industries, reflecting its role as a Strategic Cultural Zone. 77% of Southwark's creative

⁴ Southwark Employment Land Review

⁵ Shared Intelligence Preliminary Economic Assessment, 2009

industries are located in Cathedral, Riverside and Grange wards⁶.

Much of the commercial development that has occurred in recent years has been mainly located north of Southwark Street and Tooley Street where demand for large high quality modern offices is highest. This includes the More London and Bankside Mix developments.

⁶ Shared Intelligence Preliminary Economic Assessment, 2009

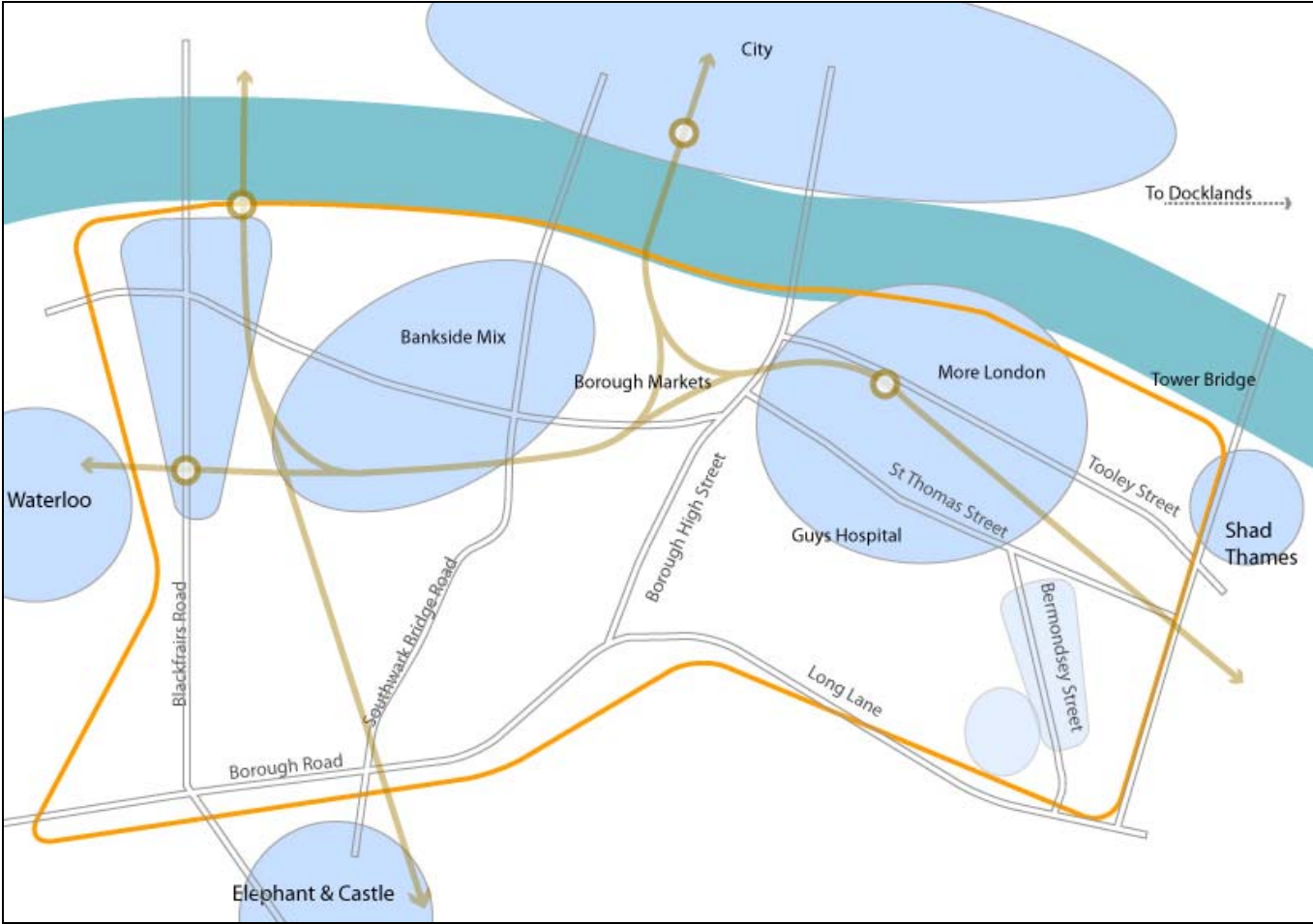


Figure A3.6: Employment and Enterprise in Bankside, Borough and London Bridge

Further south the supply is more smaller, second hand offices and studios for small businesses. There is also an emerging presence of business space in the railway arches.

A3.7 Town centre and shopping

Both Bankside and Borough and London Bridge are designated as district town centres in the Core Strategy and Southwark Plan. Together, the two centres contain over 62,000sqm of retail floorspace. However much of this is not shop (A1 floorspace). Much of the attractiveness of the area is its specialist food and leisure service offer. It is not a destination for shopping as there is a limited amount of convenience or comparison floorspace shopping.

There are retail units located throughout the area, however the main concentrations are focused around Borough High Street and Tooley Street. The Southwark Plan identifies the following as protected shopping frontages, where the mix of retail is controlled:

- Stamford Street
- Hopton Street
- The Cut
- Borough High Street
- Snowfields
- Waterloo Road
- Great Suffolk Street

Many of the retail units are used by cafes, restaurants (19%), bars/pubs (5%), takeaways (6%) or services like banks and estate agents (10%). Only 21% of units provide A1 uses and many of these mainly serve the visitor and business

population. The table below shows the breakdowns for each shopping frontage.

	Tooley Street	More London	Borough High Street	Great Suffolk Street	Bermondsey Street	The Cut	Waterloo Rd	Snowfields	Hopton Street	Stamford Street	TOTAL
A1	18%	17%	13%	56%	20%	22%	38%	31%	20%	46%	21%
A2	9%	0%	13%	6%	16%	0%	0%	8%	0%	0%	10%
A3	12%	83%	17%	22%	14%	39%	6%	31%	20%	15%	19%
A4	5%	0%	8%	6%	6%	4%	0%	15%	0%	0%	6%
A5	11%	0%	5%	0%	0%	0%	13%	0%	0%	0%	5%
B1	24%	0%	19%	0%	32%	0%	13%	0%	40%	0%	18%
B2	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%
B8	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%
C1	2%	0%	1%	0%	0%	0%	6%	0%	0%	0%	1%
C2	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%
C3	3%	0%	1%	0%	0%	0%	6%	0%	0%	0%	1%
D1	8%	0%	8%	0%	0%	4%	0%	0%	20%	8%	6%
D2	2%	0%	1%	0%	0%	0%	6%	0%	0%	8%	1%
SG	2%	0%	1%	0%	0%	4%	6%	0%	0%	0%	1%
Vacant	6%	0%	15%	11%	12%	26%	6%	15%	0%	23%	13%

With growth in businesses and homes in the area, there will be some growth in retail capacity, however the Southwark Retail Study has found that nearby centres such as Canada Water, Elephant and Castle and the West End have more capacity for growth and will continue to be the main shopping destinations in the area. It has been noted that the area currently lacks a large supermarket. This demand should be met by current approvals for large supermarkets at More London on Tooley Street and London Bridge Station on St Thomas Street. Both are yet to be completed.

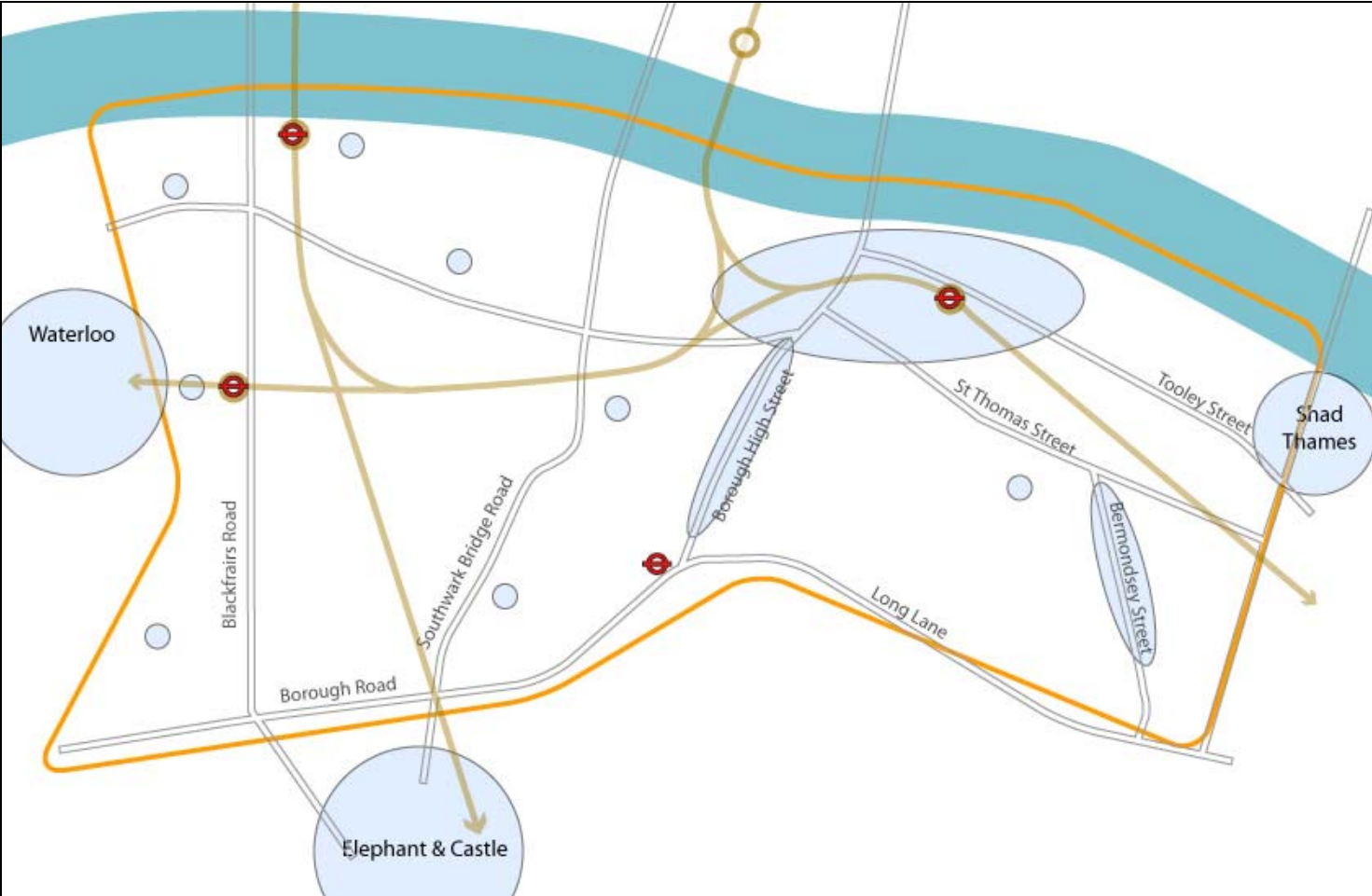


Figure A3.7: Retail areas in Bankside, Borough and London Bridge

A3.8 Arts, culture, tourism

Visitors to Southwark contribute over £700m to the borough's economy every year. The opportunity area is in the South Bank Strategic Cultural Area (see Figure A3.7) and contains a number of important arts, cultural and entertainment uses bringing thousands of people into the area each day.

These include the Tate Modern, Shakespeare's Globe Theatre, Southwark Playhouse, Unicorn Theatre Southwark Cathedral, Borough Market, the Clink Museum, London Dungeon and Southwark Playhouse. The area's many important historic places, buildings and monuments also contribute to its cultural heritage and attract visitors.

City Hall, Potters Field Park and The Scoop and are often host to public events, attracting people from across London.

The John Havard Library on Borough High Street has recently been reopened after extensive refurbishment.

The 306 metre tall mixed use tower known as 'the Shard' at London Bridge Station is currently under construction and will serve as a major landmark in the area, providing views across London from high level viewing platforms.

Alongside the major attractions a range of shopping, cafes, restaurants, pubs, bars and nightclubs have established providing both daytime and evening activity. These are focused at More London, Shad Thames, London Bridge

Station, Borough High Street, Bermondsey Street, Clink Street and along the Riverside, around the Tate Modern and at The Cut.

In November 2009 an alcohol saturation zone was introduced over much of the area in response to alcohol-related anti-social behaviour and crime.

A3.9 Hotels and visitor accommodation

The growth in arts and cultural facilities in recent years has attracted five million extra visitors a year to the area. This success, along with the growth of offices, has led to an increase in the number of hotels and apart-hotels built in the area. While this growth helps meet a need and provides employment much of it has been focused in Bankside and Borough.

The Mayor of London's Hotel Demand Study 2006 estimated that London would need an additional 40,000 hotel bed spaces by 2026, 2,800 within Southwark.

This target has almost been met. There are currently around 2,275 hotel bed spaces in the opportunity area, with a further 1,033 bed spaces approved and not yet built (see Figure A3.8). This compares with 884 new homes approved and not yet built.

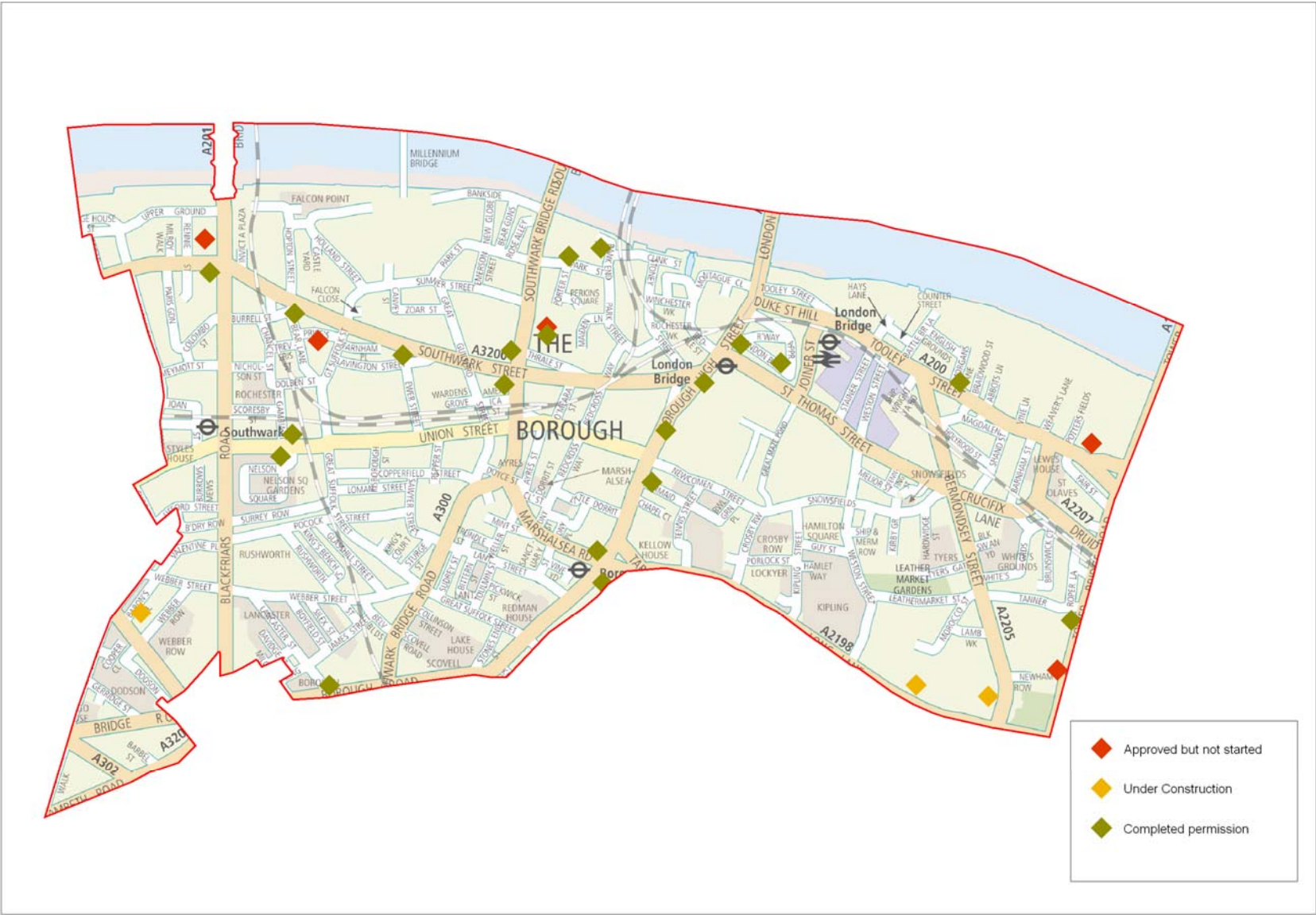


Figure A3.8: Hotel completions and approvals in Bankside, Borough and London Bridge

A3.10 Community facilities

There are a range of community facilities spread in and around Bankside, Borough and London Bridge. This includes the recently refurbished John Harvard Library, Bankside Mix Community Space, playgrounds, health clubs, leisure centres, after school clubs, youth facilities and health facilities.

Providing support and activities for young people in the area is very important. Currently, the following facilities and groups provide services to young people in the area:

- Downside Fisher.
- Rockingham Estate
- Southside
- The Hub
- The Crypt at St Judes
- St Johns
- Kids Company
- SE1 United
- Salvation Army
- Faces in Focus at Rockingham Estate
- Whites Ground Skate Park
- Colombo Sports Centre
- Millwall @ Geraldine Mary Harmsworth Park
- Southwark Sea Cadets at HMS Belfast
- Blackfriars Settlement
- Unicorn Youth Theatre
- REPA at Rockingham Estate
- Terrence Higgins Trust
- Kickstart at Rockingham Estate
- Rockingham Somali support group

The Southwark Sport and Physical Activity Strategy and Open Spaces Study show that there would be benefits to increasing

community and “pay and play” access to sports facilities, in particular swimming pools and sports halls. There are plans for new community leisure centres and swimming pools at Doon Street and Elephant and Castle which will bring benefits to the area.

There are four GP surgeries in the area as well as Guy’s and St Thomas’s hospital which provides local health services. Additional health facilities are planned in the area, including Blackfriars Extended Health Centre and Borough Extended Health Centre.

Growth in the area will increase the demand for community facilities and new development will be expected to contribute to meeting this demand,

Community facilities are shown on Figure A3.9.

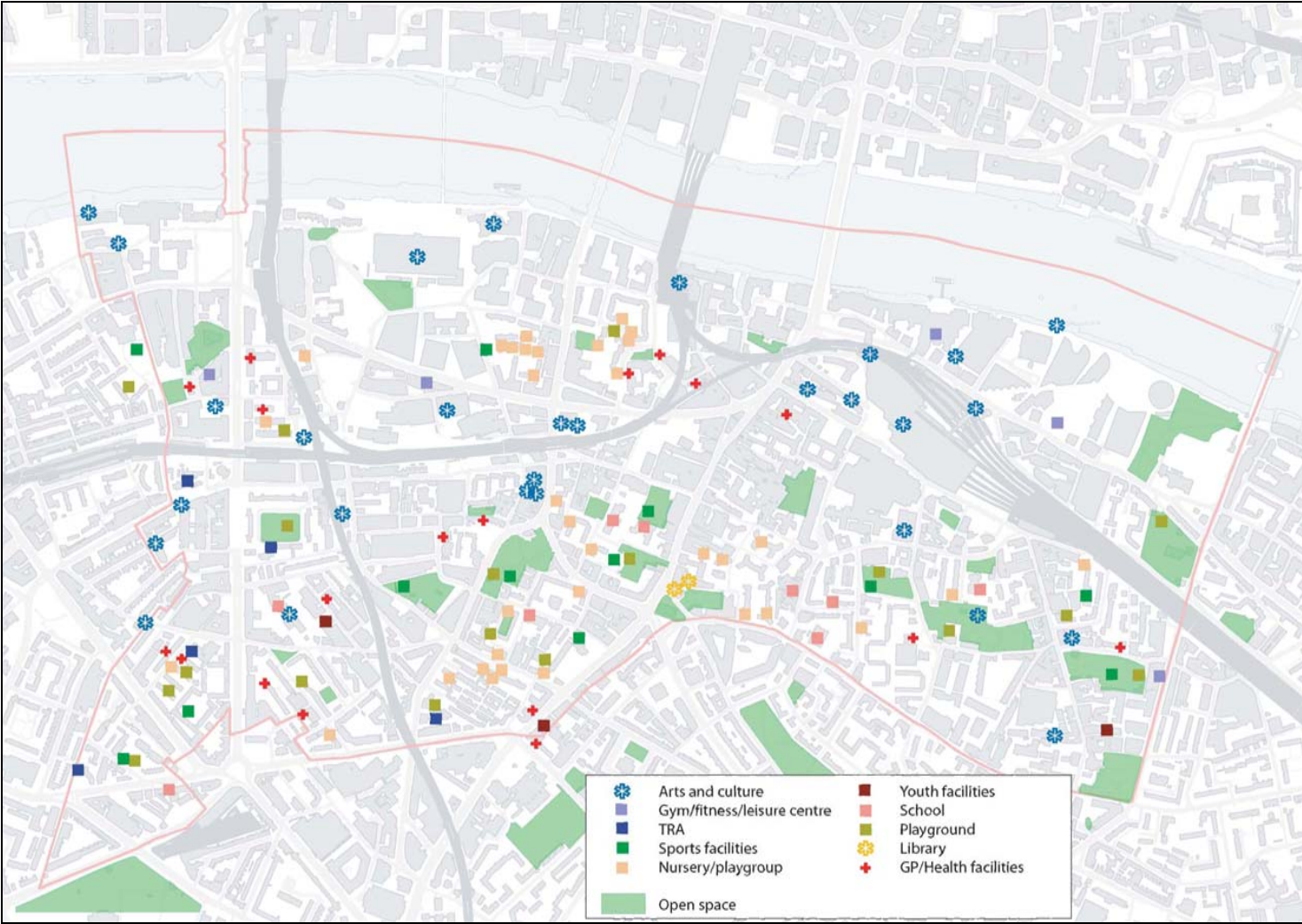
A3.11 Open spaces

The area has a network of small scattered green spaces. These spaces have a variety of character and serve a range of functions. In the north of the area along the River they provide areas of relief for visitors to relax and host events. They include Potter’s Field Park which was refurbished in 2007, the Jubilee Walk and Tate Modern Gardens.

The Thames is also an important open space and focal point in its own right, linking many important places and buildings including City Hall, Tate Modern and Southbank.

The spaces in the south of the area are quieter and serve the local population, providing a range of sports and leisure facilities. These include Leathermarket Gardens and Mint Street Park.

Figure A3.9: Community facilities in Bankside, Borough and London Bridge



There is no major park in the area, the nearest being Geraldine Mary Harmsworth Park and Southwark Park. There is also a lack of access to nature in the area as there are few accessible sites of nature conservation importance, particularly in the north of the area.

Three open spaces are designated as Sites of Importance for Nature Conservation – these are the River Thames, Leathermarket Gardens and Snowfields Nature Garden. Other green spaces including amenity areas in estates also provide habitat and opportunity for access to nature, gardening and food growing.

Many of the green spaces are well used and would benefit from improvement. This will be particularly important to help the open space network accommodate a growing population in the area.

A3.12 Environmental issues

There are important environmental issues that we need to respond to:

- **Flooding** – the area is within the River Thames flood zone (see Figure A3.10). Although it is defended to a high level, there is a small risk that the defences may fail and development needs to take this into account. The Southwark Strategic Flood Risk Assessment identified that if the flood defences were to fail, the area would be flood very quickly. There are also large areas of hard surfaces which, coupled with an old Victorian sewer system, also pose a flood risk.
- **Climate change** – is affecting everyone, but this area is particularly sensitive as it could lead to an increased flood risk. Given the high concentration of development in the area there are opportunities to reduce CO2 emissions by building energy networks. It will also be important to increase the amount of shading and greenery in the area to help adapt to hotter summers.

- **Energy efficiency** - The area is very dense with a mix of uses which means there will be a constant demand for heat and electricity throughout the day (see Figure A3.11). This makes it ideal for district energy network.
- **Access to nature** - As a very built up area there are fewer opportunities for people to experience nature and wildlife
- **Air quality** - Levels of traffic contribute to poor air quality, given vehicular demands in this area, it is located within an Air Quality Management Area.

A3.13 Drivers for change

The factors above are driving continued demand for development in Bankside, Borough and London Bridge. While there has been a lot of change in the area already, there are opportunities for Bankside, Borough and London Bridge to improve further and contribute further to a prosperous Southwark and London over the next 10 to 15 years.

The biggest opportunities are around London Bridge station and the northern end of Blackfriars Road where there are key large sites and major schemes under way, approved or being planned. These include:

- The Shard (under construction)
- Neo Bankside (under construction)
- New London Bridge Place (approved)
- Tate Modern Extension (approved)
- Redevelopment of London Bridge Station (approved, but being reviewed)
- Redevelopment and expansion of Guy's Hospital and Kings College campus (being planned)
- Potter's Field/Lambeth College (approved)

Change in the area will be supported by improvements to public transport and interchange facilities and better streets and public spaces. Projects under way or planned include:

- Potter's Field Park refurbishment (completed)
- Bermondsey Square (completed)
- Enhancements to The Cut (completed)
- Thameslink upgrade programme (under way)
- New Blackfriars Station and new entrance on the southbank (under way)
- Redevelopment of London Bridge Station and bus interchange (being planned)
- Improving Local Retail Environments (ILRE) programme (under way)
- Bankside Urban Forest (under way)
- Light at the End of the Tunnel Programme (under way)
- London Bridge BID Street Enhancement Plan (under way)
- Legible London signage project (under way)

Investment in the area will provide the opportunity for further improvements to streets, public spaces and transport.

The success of local businesses has also enabled the establishment of business improvement districts (BIDs) at London Bridge (Team London Bridge) and Bankside (Better Bankside) which are also driving improvements to the area.

Over the coming years, most of the demand for commercial floor space in Southwark will be in Bankside, Borough and London Bridge supported by excellent transport, proximity to central London and the availability of high quality office accommodation.

The Southwark Employment Land Review estimates that between 2009 and 2026 there will be demand for an extra 394,724 to 477,419sqm of office floor space in the SE1 area of Southwark. Currently there is around 270,000sqm office floor space under

construction or approved in the area. The draft replacement London Plan 2009 projects there is capacity for an extra 38,000 jobs in Southwark between 2011 and 2031, of which 25,000 will be in Bankside, Borough and London Bridge.

Continued investment in the area will help Southwark to recover faster from the current economic downturn.

There is also an opportunity for new homes to be built to help accommodate a growing population. Currently there are around 700 new homes under construction in the area. Housing capacity estimates have identified that as many as 4,000 more homes could be built in the opportunity area between 2011 and 2026 (if all possible sites are delivered)⁷. There is currently approval for 884 new homes not yet built.

⁷ Strategic Housing Land Availability Assessment of sites over 0.5ha plus Southwark Council assessment of the capacity of smaller sites.

Appendix 4: Conservation Area Appraisals

Conservation area appraisals are available on our website at:

<http://www.southwark.gov.uk/YourServices/planningandbuildingcontrol/designconservationarchaeology/CAappraisals.html>

Appendix 5: Character Area Appraisals

The character area appraisals explain the history of places and what makes them unique. They cover built form, building frontages, legibility, permeability, streetscape, public realm, landscape and open space. Each appraisal identifies buildings and features of value that contribute to each place and those that have limited value.

- Buildings identified as having townscape value should generally be retained and incorporated into development proposals.
- Buildings identified as having architectural interest, either in themselves or as part of a group, and those identified as making a positive contribution to the character of an area, will all generally be expected to be retained and reused.
- Buildings identified as having some historic interest or some urban design quality should be considered for retention and incorporated into development proposals where appropriate. Where these buildings are not retained their replacements should enhance the townscape in terms both of architecture and urban design quality.
- The character of historic building frontages should be retained and enhanced with appropriate activities.

You can download the character area appraisals from our website:
www.southwark.gov.uk/bblbspd

Appendix 6: Background to Bankside, Borough and London Bridge tall buildings study

The London Plan and Core Strategy have identified that the opportunity area is a suitable location for tall buildings and that new development could appear around the north end of Blackfriars Road and around London Bridge Station.



A detailed tall buildings study has been carried out to identify the locations where tall buildings are appropriate. The study has examined the benefits that tall buildings can bring as well as constraints such as potential effect on views and areas sensitive to tall buildings, such as conservation areas and landmarks in the area.

While tall buildings can provide a number of benefits to an area, there are sensitivities to be considered when determining whether tall buildings would be appropriate in a particular location. The study has followed the CABE and English Heritage Guidance on Tall Buildings and has considered:

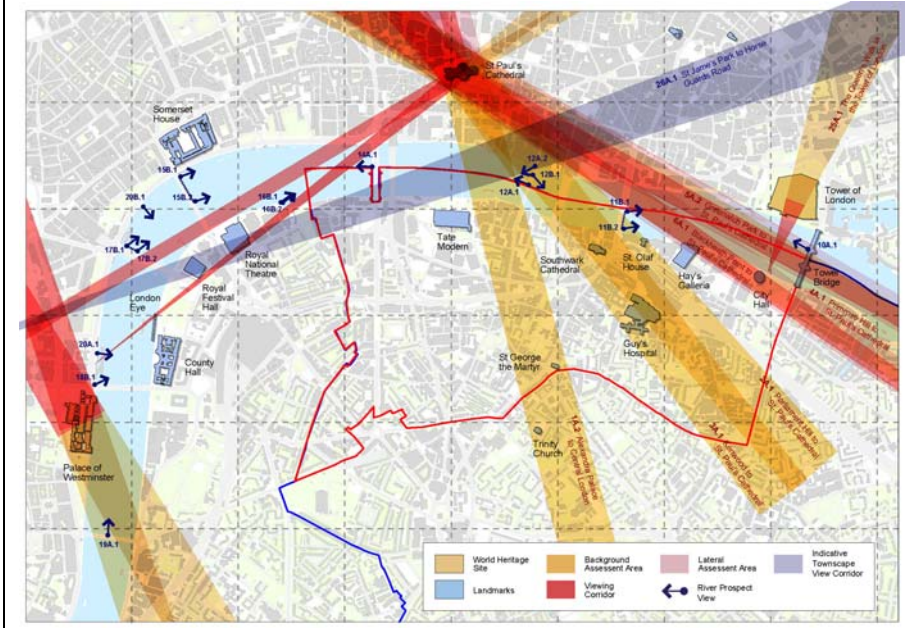
- Local character across the area, including natural topography, urban grain, scale and height, public realm, significant views of skylines and landmarks and their settings.
- Historic context, including views of World Heritage Sites and their setting, conservation areas, listed buildings, archaeology and scheduled monuments, open spaces and other important views, prospects and panoramas

Strategic Views

The Mayor of London has identified a series of public views that are important to all of London. These are views of significant important landmarks, including World Heritage Sites, that help us to understand and appreciate the city. These views are established in the London Plan and with more detailed description and guidance provided in the London View Management Framework (LVMF). The LVMF also provides guidance on how the views will be protected and how the impacts of new development will be tested.

There are a number of strategic views which are likely to be affected by development in the Bankside, Borough and London Bridge area. A full assessment of strategic views and considerations for any development in the area can be found in the tall buildings study.

Figure A6.2: Map of strategic views



- LVMF 11B.2** London Bridge - The Downstream Pavement
- LVMF 12A.1** Southwark Bridge - The Upstream Pavement
- LVMF 12A.2** Southwark Bridge - The Upstream Pavement
- LVMF 12B.1** Southwark Bridge - The Downstream Pavement
- LVMF 14A.1** Tower Bridge - The North Bastion
- LVMF 15B.1** Waterloo Bridge - The Downstream Pavement
- LVMF 15B.2** Waterloo Bridge - The Downstream Pavement
- LVMF 16B.1** The South Bank – Gabriel’s Wharf
- LVMF 16B.2** The South Bank – Gabriel’s Wharf
- LVMF 17B.1** Tower Bridge - The North Bastion
- LVMF 17B.2** Tower Bridge - The North Bastion
- LVMF 18B.1** Tower Bridge - The North Bastion
- LVMF 19A.1** Lambeth Bridge - The Downstream Pavement
- LVMF 20A.1** Victoria Embankment between Waterloo and Westminster Bridges - Opposite County Hall
- LVMF 20B.1** Victoria Embankment between Waterloo and Westminster Bridges - At Cleopatras Needle

Strategic Views - Townscape Views

- LVMF 25A.1** The Queen’s Walk to the Tower of London
- LVMF 26A.1** St James’s Park to Horse Guards Road

Views around and from within the World Heritage Sites

As identified in World Heritage Site Management Plans and local authority planning documents. Development could affect the setting of world heritage sites and this needs to be taken into account.

Important Local Views

As well as consideration of strategic views, there are views that are of more local importance.

These could be views that contribute significantly to our ability to recognise and appreciate the image and built environment of the borough and wider London. These can help us find our way around.

Strategic Views - London Panorama Views

- LVMF 1A.2** Alexandra Palace to Central London
- LVMF 2A.1** Parliament Hill to St. Paul’s Cathedral
- LVMF 3A.1** Kenwood to St. Paul’s Cathedral
- LVMF 4A.1** Primrose Hill to St. Paul’s Cathedral
- LVMF 5A.2** Greenwich Park to St. Paul’s Cathedral
- LVMF 6A.1** Blackheath Point to St. Paul’s Cathedral

Strategic Views - River Prospect Views

- LVMF 10A.1** Tower Bridge - The North Bastion
- LVMF 11B.1** London Bridge - The Downstream Pavement

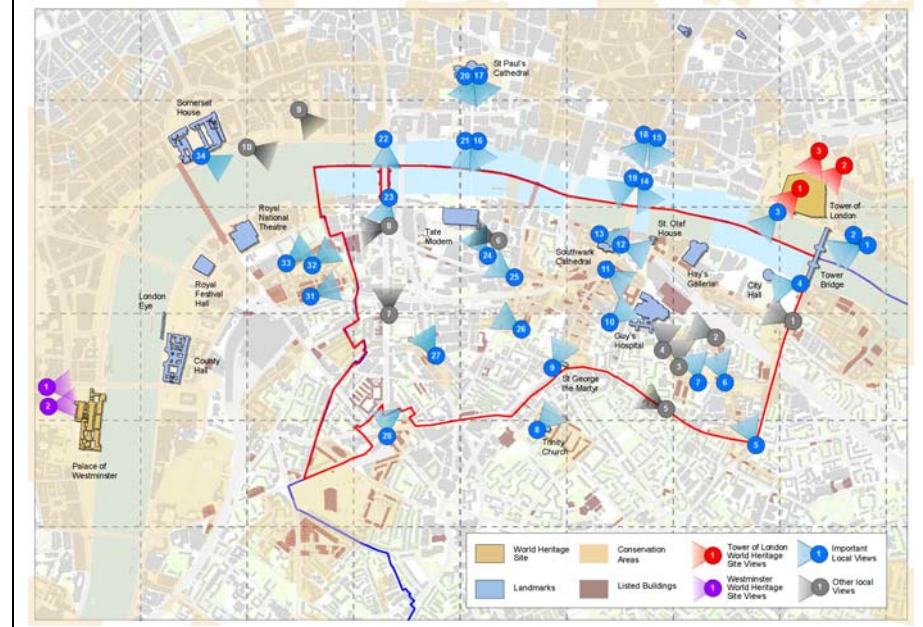
Broader or more general views which allow an appreciation of the local setting and character of an area. They also contribute to the enjoyment of open spaces and the appreciation of local landmarks and historic areas.

As part of the study of development potential in the area, a number of local views have been selected to guide the design of development in the area. These views could be from within the borough, or other views identified by adjacent boroughs, but are of local importance.

It is not the intention to protect these views as they currently are. Instead the important features of these views should be taken into account at the design stage to ensure that development makes the right contribution to improving places in Bankside, Borough and London Bridge.

Further explanation of the views assessed for this study can be found in the tall buildings study.

Figure A6.3: Map local views



World Heritage Site Views - Tower of London

- ToL 1** Tower of London - View from Inner Ward
- ToL 2** Tower of London - Mansell Street
- ToL 3** Tower of London - Tower Hill - Minories

World Heritage Site Views - Palace of Westminster

- W 1** City of Westminster Metropolitan View 37 - Houses of Parliament from Parliament Square
- W 2** City of Westminster Metropolitan View 38 - Westminster Abbey from Tothill Street/Victoria street and Broad Sanctuary

Important Local Views

IL 1	St. Katherine's Dock Entrance
IL 2	St. Katherine's Dock Entrance
IL 3	North Bank of Tower of London
IL 4	Potters Field
IL 5	Tower Bridge Road & Long Lane
IL 6	Bermondsey Street & Tanner Street
IL 7	Leathermarket Gardens
IL 8	Trinity Church Square
IL 9	Borough High Street
IL 10	Guys Hospital Inner Court
IL 11	Borough High Street & Montague Street
IL 12	Tooley Street & Borough High Street
IL 13	Montague Close
IL 14	London Bridge- North (downstream)
IL 15	City of London View 1 & 2 : Monument Viewing Gallery
IL 16	Millenium Bridge- South-East View
IL 17	St. Paul's Cathedral Viewing Gallery : View to London Bridge
IL 18	City of London View 3: Monument Viewing Gallery
IL 19	London Bridge- North (upstream)
IL 20	St. Paul's Cathedral Viewing Gallery : View to Bankside and Blackfriars
IL 21	Millenium Bridge- South-West View
IL 22	Blackfriars Bridge north end
IL 23	Blackfriars Road Head
IL 24	Great Guildford Street
IL 25	Southwark Street & Southwark Bridge Road
IL 26	Off Red Cross Way
IL 27	Kings Bench
IL 28	St George's Circus
IL 29	Old Kent Road
IL 30	Walworth Road
IL 31	Roupell Street - South side

IL 32	Aquinas Street
IL 33	Stamford Street-north pavement
IL 34	Somerset House River Terrace

Other local views

O 1	Queen Elizabeth St & Tower Bridge Rd
O 2	Crucifix Lane & Underpass
O 3	Leathermarket Street
O 4	Weston Street
O 5	Long Lane & Weston Street
O 6	Sumner Street & Emerson Street
O 7	Blackfriars Road- northward
O 8	Blackfriars Road & Southwark Street
O 9	Inner Temple Gardens - north side, base of steps
O 10	Embankment of Temple Viewing - Plaque

Appendix 7: More information on public realm projects

Bankside Urban Forest

Bankside Urban forest is a programme of works to improve the quality of the public realm and landscaping in the Bankside area, stretching from the riverside to the Elephant and Castle, and between Blackfriars Road and Borough High Street. Southwark council has worked closely with Better Bankside and the Tate to drive forward the concept of the urban forest, which sets out an approach with a 'Forest' identity to creating a network of green connections between the emerging developments in Bankside and the existing open spaces across Southwark.

Focus on breaking into the 'urban interior' of borough and opening up as more active area with finer grain, more pedestrian-friendly scale

Ensure community needs are met at key development sites
Focus on a range of 'equitable' development sites (not focusing just on riverside area) which act as catalysts for new development based on existing area characteristics.

Borough and Bankside Streetscape Improvement Programme

Approved in 2007 the Borough and Bankside Streetscape Improvement Programme will deliver 21 new public realm, open space and transport projects across the Borough and Bankside community council area, including:

- 20 mph project west of Blackfriars Road
- The Cut Lighting Scheme
- Boundary Row public realm improvements
- Styles House Boundary Treatment
- Vaughan House environs scheme
- Union Street footpath improvements
- Blackfriars Road design work
- Bankside car club

Bermondsey Streetscape Improvement Programme

Approved in 2008 the Bermondsey Streetscape Improvement Programme will deliver 31 new public realm, open space and transport projects across the Bermondsey community council area.

London Bridge BID Street Enhancement plan

Development of new pocket parks to compliment existing in area to provide small-scale public space to people who work in/around London Bridge District

Ensure that pedestrian movement is prioritised along key corridors such as Tooley St, St Thomas' street and Northern end of Borough High St

Lighting

Lighting has been upgraded in many of the roads within the framework area over the past few years, or is due for replacement during 2009. Such upgrades have been delivered through a series of Environmental improvements such as Light at the End of the Tunnel, through projects like The Cut Refurbishment scheme; or have been delivered through the Councils capital budgets.

Light at the End of the Tunnel

Light at the End of the Tunnel is transforming 10km stretch of dark and forbidding railway viaducts into light and safe passageways. Running from Vauxhall to Bermondsey the railway viaducts forms one of the biggest barriers to economic development and job creation in some of the most deprived areas of the country. LET has invested funds to clean, light, commission new public art for the viaducts to transform the pedestrian experience and making the viaducts viable, safe and functional spaces. Upgrading the viaduct and public spaces surrounding them helps to unleash investment potential in the area. It also encourages businesses to take

advantage of the 1000 adjoining arches using them as commercial space.

Our approach is to upgrade lighting from older off white or yellow sodium lighting to using white light throughout the borough, even where current lighting standards are met.

There are nine streets where lighting improvements are recommended to white light giving the complete area a uniform level and colour of light. These are:

- Blackfriars Road
- Borough Road
- Colombo Street
- Rennie Street
- Rushworth Street
- Surrey Row
- Valentine Row
- Westminster Bridge Road
- St Georges Circus.

Signage

Southwark Council undertook a review of signage in Bankside in 2005 and instigated enhancements to the signage in the framework area including a number of finger posts and improvements to the Tate Tube pedestrian route. South Bank Employers Groups have also undertaken some signage work in the area. The signage is positioned to the North of The Cut and Union Street junction of Blackfriars Road and there is opportunity to improve signage to the south of this area as well as opportunities for continuity of signage system and wayfinding as opposed to direction signage through Legible London.

Southwark has a signage strategy for all levels of signage for the Borough which should be adhered to through out the framework area.

Create robust signage and advertising policies to reduce clutter and protect the character of the Area

Legible London

Legible London is a scheme to provide better information throughout the capital for people who want to walk. By coordinating information across the boroughs, the different transport modes, and the media people use to find their way around, Legible London will improve and support 'mental mapping' of London, ensuring that Londoners and visitors alike will be more confident about travelling around the capital on foot. The programme will help to deliver the Mayor's vision of making London a world-class walking city by 2015 and provide important support for the influx of visitors expected to attend the 2012 Olympic Games and is being introduced on a trial basis in South Bank and Bankside. Approximately half of the Blackfriars area, i.e. The Cut northwards, falls within the pilot area for Legible London.

Stoney Street/Winchester Walk transport and streetscape improvement scheme

The scheme Borough Markets and the surrounding streets and will deliver a transport improvement and public realm improvement project, including:

- Periodic closure of Stoney Street to motor vehicles between 10am to 6pm on Thursdays, Fridays and Saturdays
- Improvements to the operation of the Stoney Street and Winchester Walk junction
- Improvements to public realm with specific emphasis on pedestrian and cycle movement.
- A review of parking and loading/unloading arrangements, waste management and enforcement regimes.

Appendix 8: More information on Employment, Skills and Training schemes

Building London Creating Futures aims to find long-term employment for jobseekers in London while simultaneously working with the construction industry to develop recruitment and training practices that reflect the changing needs of the sector.

The START arts and culture Workplace Co-ordinator (WPC) programme aims to help unemployed people get customer facing jobs with many exciting employers in central London, including Tate, Imperial War Museum, National Theatre and the London Eye.

Southwark Works has an established team of 17 specialist employment advisers, who work closely with JobcentrePlus, mainstream agencies and a range of other employment and training related partners. The advisers are located with a variety of service providers, each trying to tackle specific barriers to the labour market and help those at most disadvantage – throughout the borough.

Blackfriars Settlement through its Education and Training Centre delivers a full timetable of Adult Literacy and ESOL (English for Speakers of Other Languages), as well as an Employment Skills Training program aimed at enabling people to successfully navigate an increasingly complex job market and secure employment.

Business Support Services and Enterprise Development

Smaller businesses in particular are able to access a number of business support services provided by the Council and business support initiatives. These include **Business Extra** and **GLE One London**, both based in close proximity to the Blackfriars area and providing advice to start-up and SME businesses, together with the Council's business desk.

The **Southwark Business Desk** is a referral service that is managed by the Economic Development Team and provides information for any business related enquiry.

- **Business Extra**, have been contracted to deliver the *Success in Southwark* project which is a business information support service that also helps businesses find the right services they are looking for.

Appendix 9: Glossary

Accessibility is defined as the methods by which people with a range of needs find out about and use services and information. For disabled people, access in London means the freedom to participate in the economy, in how London is planned, in the social and cultural life of the community, as well as physical access. The term is also used to describe how easy a location is to get to by transport.

Active Street Frontages Making frontages 'active' adds interest, life and vitality to the public realm. Active frontage should consist of the following:

- Frequent doors and windows, with few blank walls
- Articulated facades with bays and porches
- Lively internal uses visible from the outside, or spilling onto the street

Activity node – concentration of activity at a particular point

Affordable housing Housing designed to meet the needs of households whose incomes are not sufficient to allow them to access decent and appropriate housing in their borough. Affordable housing comprises social and intermediate housing.

Amenity Pleasant or advantages features enjoyed by a property or occupier, which can be affected by certain development proposals. Development can improve amenity as well as harm it.

Archaeological Priority Zones The boundaries of Archaeological Priority Zones are designated on the Proposals Map. These are areas where there is potential for significant archaeological remains, and planning applications within these areas must be accompanied by an archaeological assessment and evaluation of the site, including the impact of the proposed development.

Biodiversity The diversity or variety of plants and animals and other living things in a particular area or region. It encompasses landscape diversity, eco-system diversity, species diversity, habitat diversity and genetic diversity.

Borough Open Land Borough open land is open space of local importance and is designated in the Southwark Plan. Borough Open Land must meet all of the following criteria:

- An area of local importance to Southwark;
- A clearly distinguishable public open space;
- Land that contains features or landscapes of historic, recreational or nature conservation value at a borough level;
- It must not be Metropolitan Open Land.

Building Research Establishment Assessment Method (BREEAM) Measures the environmental performance of commercial buildings by assessing water, waste, energy and travel usage

Buildings at Risk Any building included in 'Buildings at Risk: The Register (London)', published by English Heritage. These are buildings that are at risk of being damaged or lost due to deterioration or decay.

Brownfield Any land or premises which has previously been used or developed and is not currently fully in use, although it may be partially occupied or utilised. It may also be vacant, derelict or contaminated.

Central Activity Zone (CAZ) The Central Activity Zone is the area within Central and East London where planning policy promotes finance, specialist retail, tourist and cultural activities.

“CHP” or Combined Heat and Power CHP describes plant that is designed to produce both heat and electricity from a single heat source.

Code for Sustainable Homes (CfSH) Code for Sustainable Homes is a new national standard to guide the design and construction of sustainable homes. The Code gives a sustainability rating to development which ranges from 1(*) to 6(*****). The higher the rating the more sustainable a home is. The assessment includes efficiency in energy, water, waste, materials, ecology and surface water run-off.

Community facilities These are defined by the Town and Country Planning (Use Classes) Order 1987 and its subsequent amendments:

- **Non-residential institutions** - Clinics, health centres, crèches, day nurseries, day centres, schools, art galleries, museums, libraries, halls, places of worship, church halls, law court. Non residential education and training centres.
- **D2 Assembly and leisure** - Cinemas, music and concert halls, bingo and dance halls (but not night clubs), swimming baths, skating rinks, gymnasiums or sports arenas (except for motor sports, or where firearms are used).

Conservation Areas An area of special architectural or historic interest designated by the local planning authority under the provisions of the Planning (Listed Buildings and Conservation areas) Act 1990, the character or appearance of which it is desirable to preserve or enhance.

Context The setting of a site or area, including factors such as traffic, activities and land used as well as landscape and built form.

Controlled Parking Zones (CPZ) Areas in which on-street parking is managed, usually through a permit system, typically where demands for limited space from residents, commuters, shoppers and others are in conflict.

Core strategy A development plan document. Sets out the key elements of the planning framework for an area, comprising a spatial strategy, strategic policies and a monitoring and implementation framework with clear objectives for achieving delivery. All other development plan documents should be in conformity with the core strategy.

Creative and cultural industries The Department for Culture Media and Sport (DCMS) classifies the following industries as part of the CCI sector, “advertising, architecture, the art and antiques market, crafts, design, designer fashion, film and video production, interactive leisure software, music, the performing arts, publishing, software and computer services, and television and radio production”.

Density The floor space of a building or buildings or some other unit measure in relation to a given area of land. Built density can be expressed in terms of plot ratio (for commercial development); number of units or habitable rooms per hectare (for residential development); site coverage plus the number of floors or a maximum building height; or a combination of these.

Development As defined by Section 55 of the Town and Country Planning Act 1990, development means carrying out building, engineering, mining or other operations in, on, over or under land, or changing the use of buildings or land.

Development Plan Document (DPD) A spatial planning document prepared by a plan-making authority and subject to independent examination.

Diversity The differences in the values, attitudes, cultural perspective, beliefs, ethnic background, sexuality, skills, knowledge and life experiences of each individual in any group of people. This term refers to differences between people and is used to highlight individual need.

Employment uses Uses falling within Class B1, Class B2 and Class B8 of the Use Classes Order. These include offices, factories and warehouses (See Use Classes Order).

Energy Efficiency Making the best or most efficient use of energy in order to achieve a given output of goods or services, and of comfort and convenience. This does not necessitate the use of less energy, in which respect it differs from the concept of energy conservation.

Environment Surrounding area or conditions in which something exists or lives.

Floor space The sum of the area of all floors (both above and below ground) of all buildings on a site, usually measured from the outside walls on every floor.

Growth Areas These are areas best able to accommodate major growth in jobs and housing and include the opportunity areas of London Bridge, Borough and Bankside, Elephant and Castle, and the action area cores of Canada Water, Aylesbury and Peckham. These areas have better infrastructure, especially transport links

Habitat The environment required to sustain animals, plants and other species. It includes air, water and soil, as well as other living things.

Height The height of a building can be expressed in terms of a maximum number of floors; a maximum height of parapet or ridge; a

maximum overall height; any of these maximum heights in combination with a maximum number of floors; a ratio of building height to street or space width; height relative to particular landmarks or background buildings; or strategic views.

Hierarchy of town centres This is a ranking of town centres based on size. It is used to ensure that the largest developments are directed to major centres that have the infrastructure to accommodate them, rather than smaller district or local centres that serve more local communities.

Heritage The evidence of the past, such as historical sites, buildings and the unspoilt natural environment, considered collectively as the inheritance of present-day society.

Infrastructure This includes transport, health, schools and social services facilities as well as energy and water supply. Major developments should not go ahead without the necessary infrastructure to meet the needs of new residents or workers.

Intermediate Housing made available to those households who cannot be accommodated within the capacity of the social housing provision, and cannot access market housing. This type of accommodation is often made available to key-workers and other lower to medium income groups, and is typically provided through low-cost housing schemes such as shared ownership.

Landmark A building or structure that stands out from its background by virtue of height, size or some other aspect of design

Landscape The character and appearance of land, including its shape, form, ecology, natural features, colours and elements and the way these components combine. Landscape character can be expressed through landscape appraisal, and maps or plans.

Layout The way buildings, routes and open spaces are placed in relation to each other.

Lifetime Homes Are ordinary Homes designed to accommodate the changing needs of occupants throughout their lives, particularly loss of mobility.

Local Development Framework (LDF) A portfolio of documents including Supplementary Planning Documents, Development Plan Documents such as a Core Strategy and proposals maps, and the Statement of Community Involvement. Together the portfolio of documents covers the whole borough and gives comprehensive policy coverage at a detailed level.

London Plan A strategic plan for the whole of London produced by the GLA. The main purpose of the London Plan is to ensure that all the individual plans produced by the London boroughs work together to meet the priorities that are agreed for the whole London region. The Local Development Framework cannot contain strategies or policies that are not in general conformity with the London Plan.

Major developments Any residential or mixed use development creating 10 or more dwellings, or if that is not known, where the site area is 0.5 hectares or more. For other types of development, a major development is one where the floorspace to be built is 1000 square metres or more, or the site area is 1 hectare or more.

Massing The combined effect of the height, bulk and silhouette of a building or group of buildings.

Metropolitan Open Land Strategic open land within the urban area that contributes to the structure and quality of life of Londoners. There is a strict control on building in metropolitan open land

(designated in the London Plan) similar to the controls on Green Belts.

Mixed use Development comprising one or more activities within the same building or site, or across sites in close proximity to one another.

Movement People and vehicles going to and passing through buildings, places and spaces.

Multi Utility Services Company (MUSCo) A MUSCo is a private/public joint venture energy and environmental services company that delivers several utilities in a sustainable way. There is one planned at Elephant and Castle and Aylesbury that will supply low carbon energy and environmental services, including non-potable water and next generation data connectivity.

Nature Conservation Protection, management and promotion for the benefit of wild species and habitats, as well as the human communities that use and enjoy them. This also covers the creation and re-creation of wildlife habitats, the techniques that protect genetic diversity and can be used to include geological conservation.

Open Space The term open spaces covers all land use in London that is predominantly undeveloped other than by buildings or structures that support the open space use. This definition covers a range of types of open space within the urban area, both in public or private ownership and whether access is unrestricted, limited or restricted.

Opportunity Areas Locations within London capable of accommodating large scale development to provide substantial numbers of new employment and housing, typically more than 5,000 jobs and/or 2,500 homes, with a mixed and intensive use of

land and assisted by good public transport accessibility. In Southwark, Borough, Bankside and London Bridge and the Elephant and Castle have been identified as Opportunity Areas.

Planning obligations These are also called planning gain or section 106 contributions. Applicants for major developments are usually required to provide payments to offset the impact of their scheme and make it acceptable. This may include improved transport, environment, employment, housing or recreation facilities. They could also be built as part of a development rather than provided as a payment.

Preferred Industrial Location (PIL) They are areas with a concentration of employment uses which are of importance to the Borough's or London's economy and provide an importance source of local employment opportunities.

Public Realm The space between and within buildings that is publicly accessible or visible, including streets, squares, forecourts, parks and open spaces.

Public Transport Accessibility Level (PTAL) This measures ease of access to the public transport network and is based on distance to public transport and frequency of service. PTALs range between 0 and 6A, with 0-2 comprising a low PTAL, 3-4 a medium PTAL and 5-6A a high PTAL.

Public Transport Accessibility Zones These are areas within the Urban or Suburban Zones, which have better access to public transport, significant potential for new development and investment, and a mixed use character including significant retail development. The four Public Transport Accessibility Zones are Canada Water, the Walworth Corridor, Bermondsey Spa and Peckham.

Regeneration Lasting improvements to the economic, social and environmental conditions of an area and/or community that has been subject to urban deprivation.

Renewable energy Energy derived from a source which is continually replenished, such as wind, wave, solar, hydroelectric and energy from plant material. Although not strictly renewable, geothermal energy is generally included.

Secured by Design A police initiative to encourage the building industry to adopt crime prevention measures in development design to assist in reducing the opportunity for crime and the fear of crime, creating a safer and more secure environment.

Scale The impression of a building when seen in relation to its surroundings, or the size of parts of a building or its details, particularly as experienced in relation to the size of a person. Sometimes it is the total dimensions which give it its sense of scale and at other times it is the size of elements and the way they are combined.

Sites of Importance for Nature Conservation A series of sites identified to represent the best wildlife habitats and emphasise the value of access for people. Sites are classified into Sites of Metropolitan, borough and Local Importance for Nature Conservation.

Small Business Units Business units with a floorspace under 235 square metres.

Social Rented (Social Housing) Affordable housing which is affordable by all those in housing need. This is typically provided as rented accommodation through the local authority or a Registered Social Landlord and rents that can be charged are set by the Government.

Social Infrastructure includes healthcare and social care, safety and security, policing facilities, public realm, play space and open space, inclusive design and local distinctiveness, community engagement access to employment/skills development opportunities and the provision of suitable space for small and medium-sized enterprises.

Southwark Plan Southwark Plan is the main set of planning policies for the borough, also known as the Unitary Development Plan (UDP), and this forms the core of the local development framework (LDF). It sets out a vision and policies for the sustainable use and development of land through protection and regeneration to deliver the Community Strategy over the next 10 years. The policies are used to determine applications for planning permission in the borough, along with the London Plan and other planning documents, such as supplementary planning documents.

Strategic Cultural Areas Parts of the borough thought most suitable for new art, cultural, and visitor attraction uses to be created.

Strategic Flood Risk Assessment (SFRA) This looks at how Southwark is at risk from flooding, including from the River Thames and drainage overflowing.

Strategic Housing Land Availability Assessment The SHLAA is a major piece of work being carried out by the GLA with the help of the boroughs. It aims to identify potential housing sites that could be brought forward over the next 20 years to enable councils to meet their obligations to increase the housing stock and meet housing need. It covers all sites over 0.25 hectares

Supplementary Planning Document (SPD) Supplementary Planning Document (SPD) expands upon and provides further guidance on how planning policies in the Southwark Plan should be

applied. Once adopted, an SPD will become a material consideration when considering planning applications in Southwark and is regularly monitored and reviewed.

Sustainable Development Development that contributes towards the principles of sustainability. That is, development that does not cause environmental damage, contributes to the local economy and meets the needs of the local community.

Sustainable Urban Drainage Systems (SUDS) Techniques for dealing with problems of flooding and surface water quality. They can consist of a variety of measures to recycle, dispose of and reduce surface water. Infiltration and reduction could be provided by a variety of means including porous paving, oversized pipes, cellular storage tanks, green or brown roofs.

Sustainability Appraisal/ Strategic Environmental Assessment

A systematic and interactive process undertaken during the preparation of a plan or strategy, which identifies and reports on the extent to which the implementation of the plan or strategy would achieve environmental, economic and social objectives by which sustainable development can be defined, in order that the performance of the strategy and policies is improved.

Sustainable Transport Alternative modes of transport to the low-occupancy private car, including walking, cycling, public transport, car sharing, water transport and city car clubs.

Thames Policy Area (TPA) An area along the River Thames identified in the local development framework. Special policy requirements apply in this area to make sure new development protects and improves the river and the character and quality of the public realm along the river.

Town Centres Areas that provide a broad range of facilities and services and act as a focus for both the community and for public transport. It excludes small parades of shops of purely local significance.

Transport Assessments Major developments need to provide an assessment of the likely increase in traffic or pedestrian/ public transport movements arising from the scheme and what measures will be taken to mitigate any negative effects, e.g. congestion or pollution. A Travel Plan, outlining sustainable transport objectives, targets and initiatives will be expected to be included within all Transport Assessments.

Wheelchair affordable housing – This refers to homes built to meet standards set out in National Wheelchair Housing Group report Wheelchair Housing Design Guide 1997.

Unitary Development Plans (UDPs) Statutory plans produced by each borough which integrate strategic and local planning responsibilities through policies and proposals for the development and use of land in their area.